

LAMBERT/IDAHO SPECIFIC PLAN  
CITY OF LA HABRA

MARCH 2004





# LAMBERT/IDAHO SPECIFIC PLAN

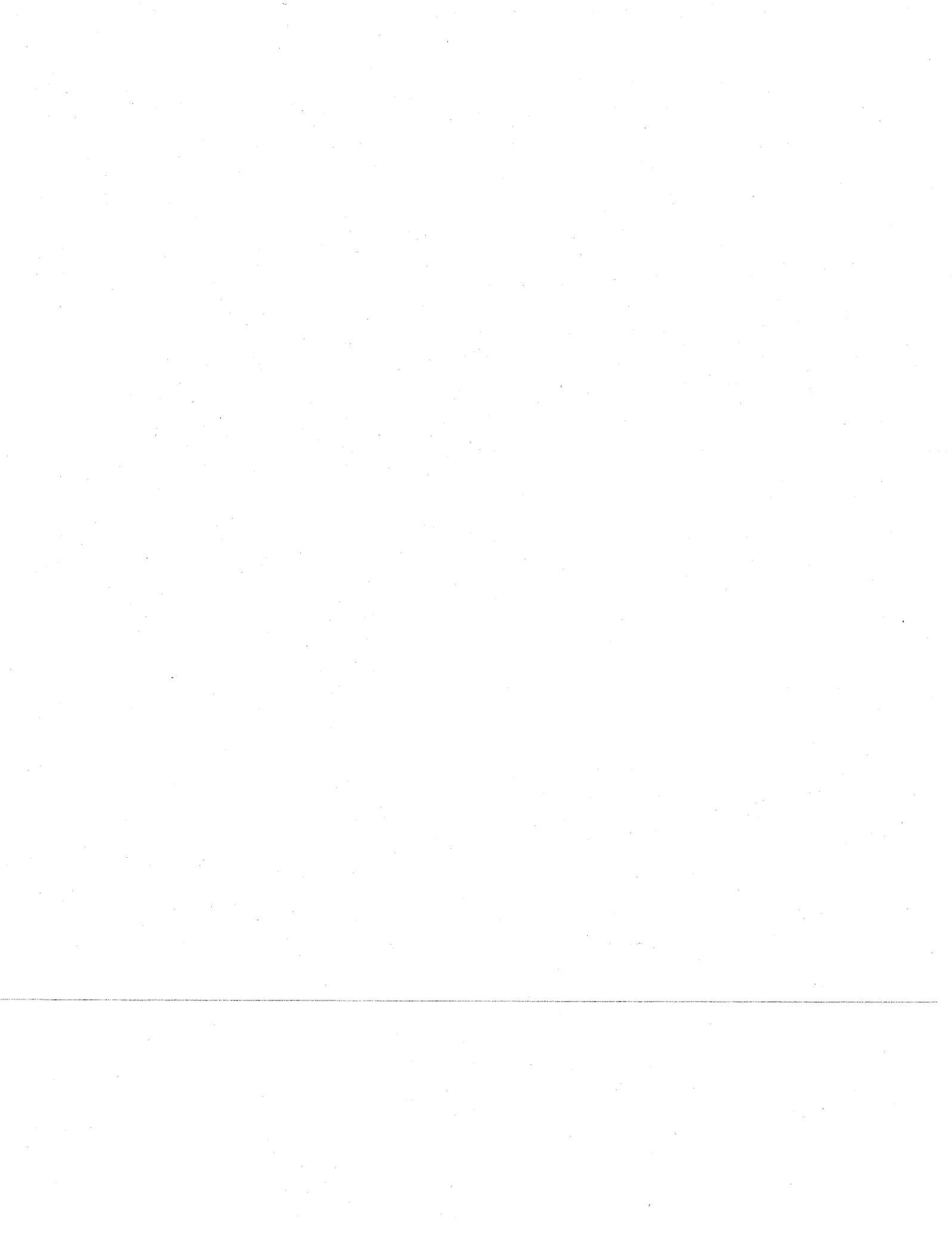
## CITY OF LA HABRA

MARCH 2004

The Olson Company

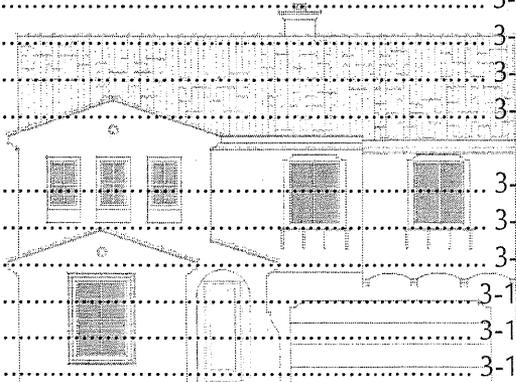
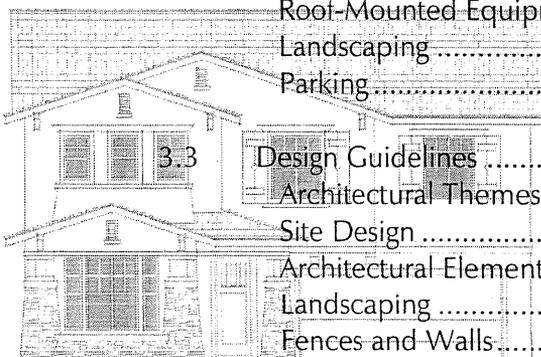
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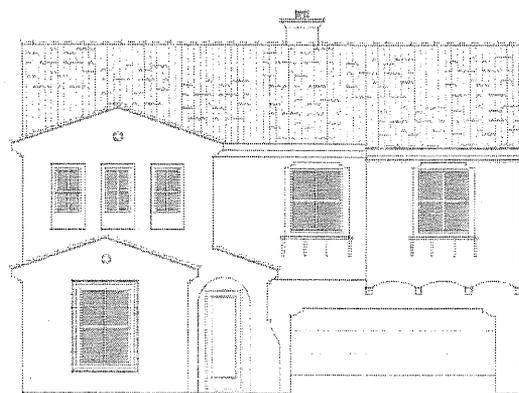
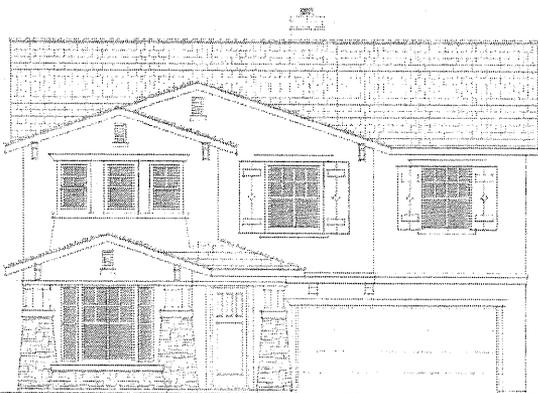
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# INTRODUCTION

The Lambert/Idaho Specific Plan presents an opportunity for quality residential infill development to be built on a vacant site in the City of La Habra. This Plan provides for the reuse of the Friendly Hills Medical Facility site in a manner that complements surrounding land uses. The project offers a development plan for an 18.6-acre master-planned residential neighborhood. The project applicant, the Olson Company, proposes development of up to 112 detached single-family homes and 1.25 acres of on-site active recreation areas and open space.

## 1.1 LOCATION

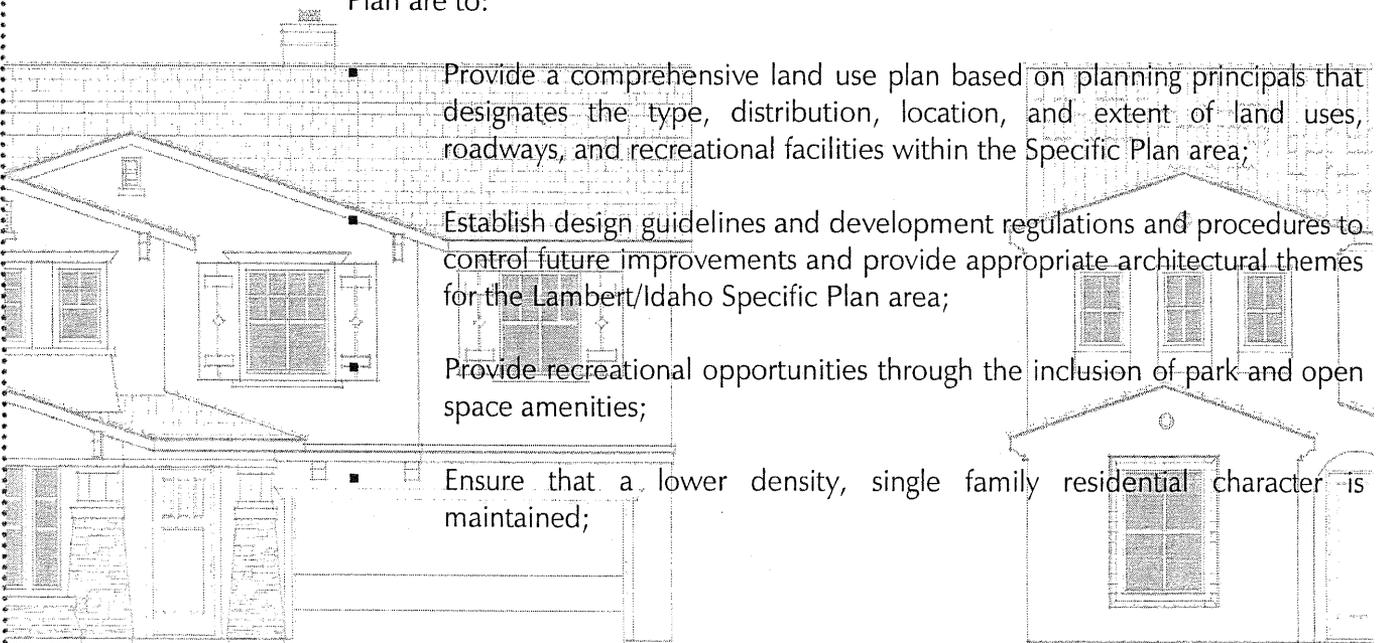
The project site is located within the City of La Habra in north Orange County, at the northwest corner of Lambert Road and Idaho Street. The site is bounded by Idaho Street on the east, Lambert Road on the south, a partially channelized drainage course on the west, and a Southern Pacific Railroad right-of-way on the north. Commercial properties are adjacent to the site's northeast and southwest corners, and other surrounding uses consist largely of residential development. Figure 1 shows the location of the project site. Both Lambert Road and Idaho Street provide direct access to the site.

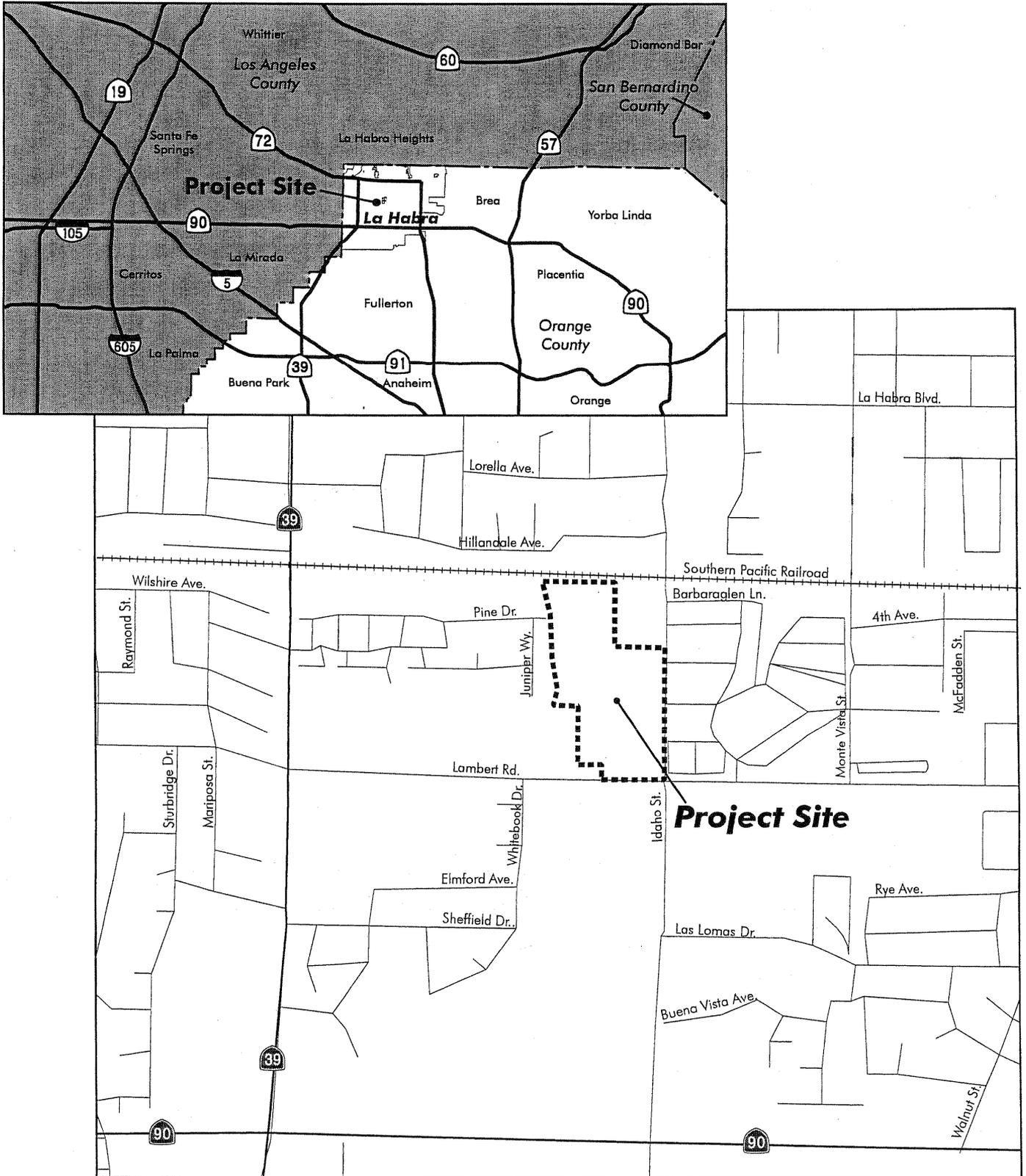
## 1.2 GOALS AND OBJECTIVES OF THE SPECIFIC PLAN

This Specific Plan defines land use regulations, development standards, and architectural guidelines for all new development on the site. The Plan is a regulatory tool that the City, property owners, and development community will use to achieve neighborhood enhancement. The Plan replaces standard City zoning regulations applicable to the area, as well as redesignates General Plan land uses.

The goals and objectives established for the project in preparation of the Specific Plan are to:

- Provide a comprehensive land use plan based on planning principals that designates the type, distribution, location, and extent of land uses, roadways, and recreational facilities within the Specific Plan area;
- Establish design guidelines and development regulations and procedures to control future improvements and provide appropriate architectural themes for the Lambert/Idaho Specific Plan area;
- Provide recreational opportunities through the inclusion of park and open space amenities;
- Ensure that a lower density, single family residential character is maintained;





Source: William Hezmalhalch Architects, Inc.,  
Cotton/Bridges/Associates, November 2003

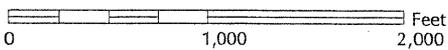


Figure 1  
Regional Location

# INTRODUCTION

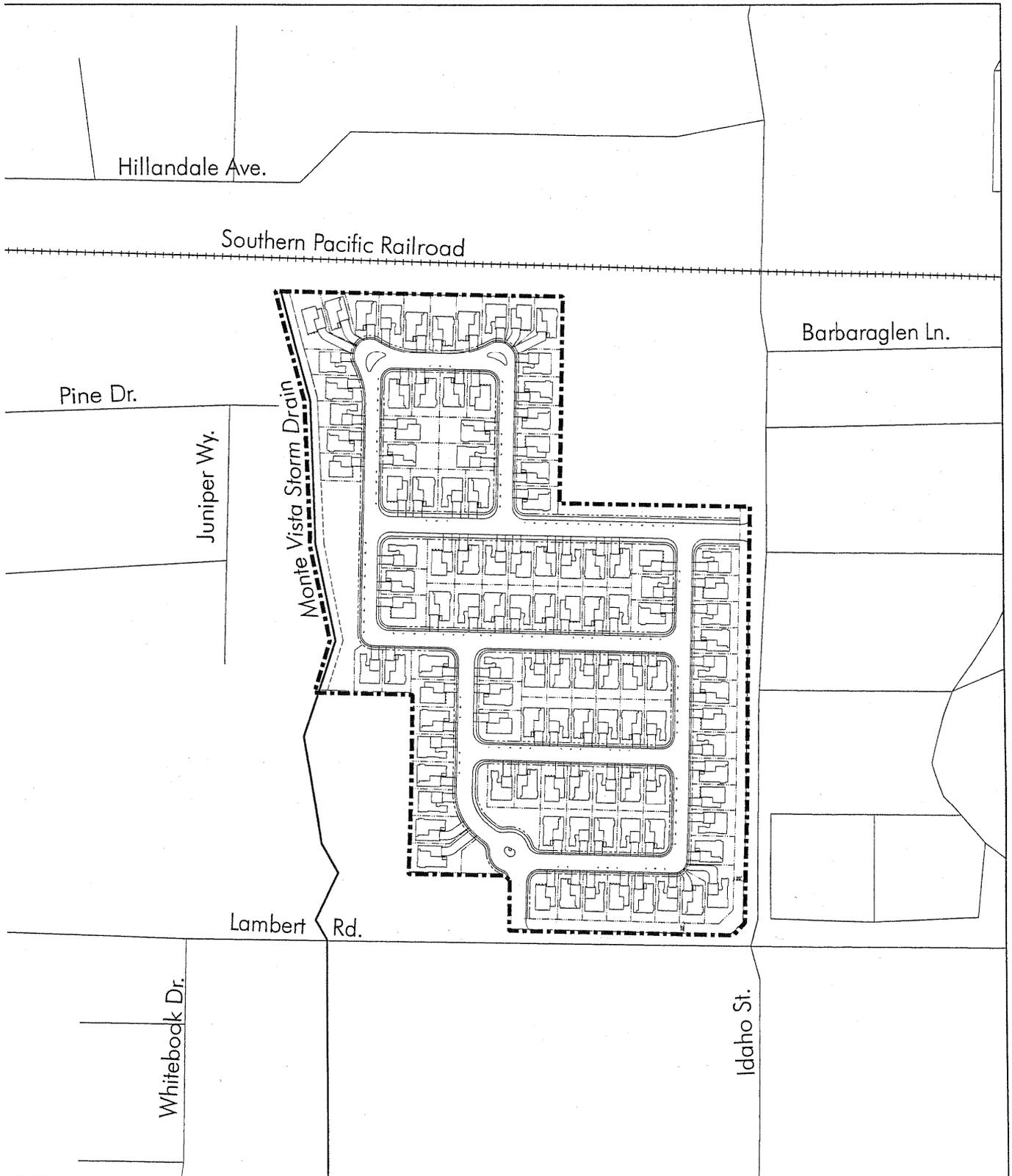
- Provide adequate infrastructure improvements required to support the land use envisioned;
- Provide for long term maintenance of all improvements by a Homeowners' Association and/or through Covenants, Codes, and Restrictions (CC&Rs); and
- Identify strategies that will implement Plan concepts.

## 1.3 AUTHORITY AND SCOPE

California Government Code Section 65450 et seq. authorizes preparation of specific plans to implement a jurisdiction's General Plan. State law provides limited guidance regarding the content and scope of a specific plan, indicating that a specific plan must address the distribution, location, and extent of land use; the infrastructure – both public and private – necessary to support the land use plan; standards and criteria for natural resource preservation, where applicable; and an implementation plan that includes identification of financing measures. Also, the specific plan must include a statement of the relationship of the specific plan to a communities General Plan. The specific plan may also address any other subjects which decision makers find necessary or desirable to achieve city goals, as stated in the General Plan.

This Specific Plan reflects all state requirements. Following this introduction, Part 2.0 addresses the Development Setting and Concept, describing the site in detail and discussing the proposed development type. Part 3.0 next outlines specific development standards, land use regulations, and design guidelines which will regulate all future land uses on this site. Part 4.0 addresses infrastructure as it exists and what is needed to implement the proposed land uses. Part 5.0 addresses how community services such as police and fire protection will be provided on the site. Part 6.0 outlines specific implementation and administrative measures to ensure that all provisions of the specific plan are carried out.

The regulations contained in this Specific Plan apply to all properties within the project area identified in Figure 2. The regulations contained herein take precedence over the zoning regulations contained in Title 18 of the City of La Habra Municipal Code. Following adoption of the Lambert/Idaho Specific Plan by the City Council, all subsequent development within the project area must be consistent with Specific Plan regulations and standards. This includes all local public works projects, tentative maps, and parcel maps (Government Code Section 65455). Where a particular standard is not addressed in this Specific Plan, the regulations of Title 18 shall apply.



Source: William Hezmalhalch Architects, Inc.

 Specific Plan Area

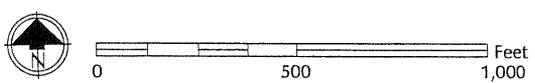


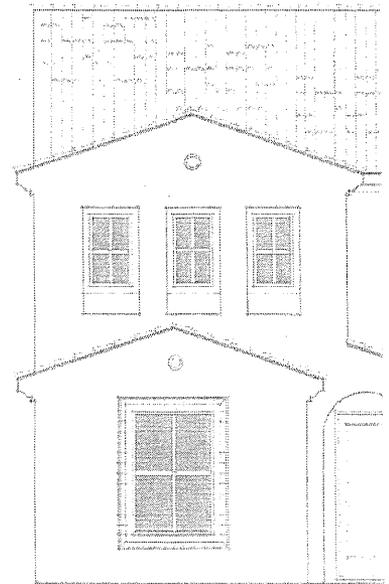
Figure 2  
Specific Plan Area

# INTRODUCTION

## 1.4 ORGANIZATION OF THE SPECIFIC PLAN

This Specific Plan defines land use regulations, development standards, and architectural guidelines to allow for the creation of a residential neighborhood. The Plan consists of text, maps, and diagrams that describe:

- Overall land use plan and development concept for the project site
- Roadway and Circulation plan
- Plans for open space, landscaping, and recreation facilities
- Plans for constructing and maintaining public infrastructure
- Development standards for residential uses (e.g. required setbacks, maximum building height, and landscape requirements)
- Design guidelines





# DEVELOPMENT SETTING AND CONCEPT

## 2.1 DEVELOPMENT SETTING

The Friendly Hills Medical Center formerly occupied the site. The site is now vacant; all buildings on the project site that were associated with the medical center have been demolished. The remainder of the site consists of surface parking lots, sparse landscaping, and a small portion of a flood control channel (Monte Vista Storm Drain). These uses are illustrated in Figure 3.

## EXISTING LAND USES

## EXISTING CIRCULATION

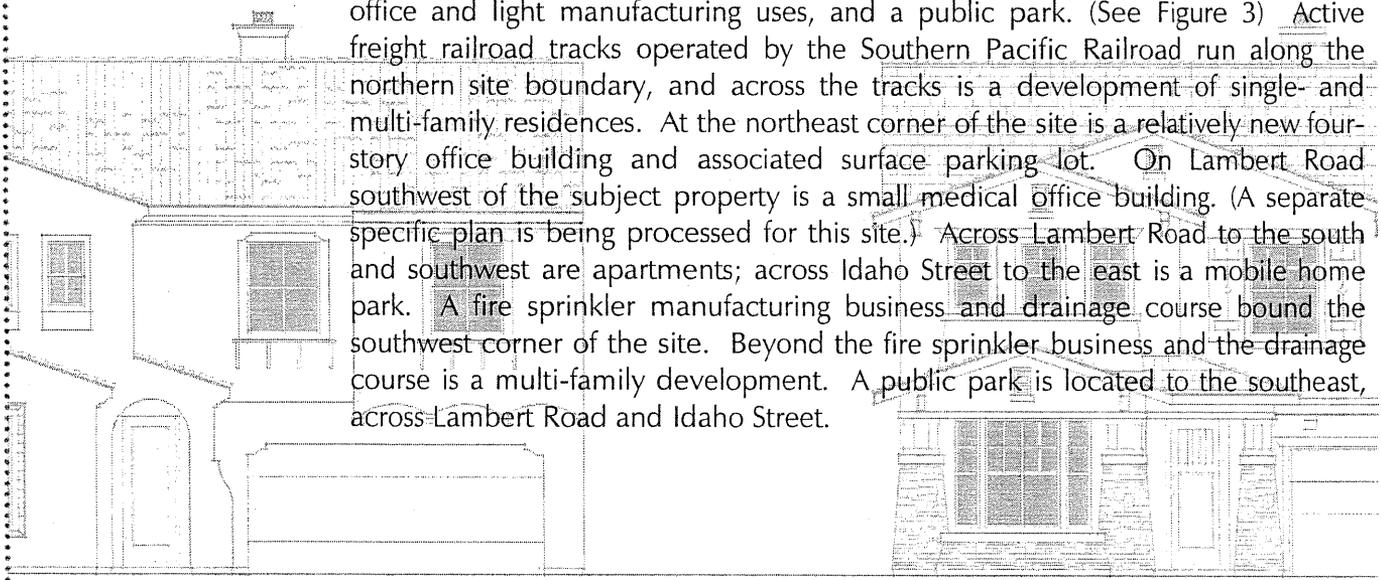
The site is accessed from both Lambert Road, a four-lane east-west median divided roadway, and Idaho Street, a four-lane north-south undivided roadway. The project site plan provides for ingress/egress from both of these streets. The closest signalized intersection is at Lambert Road and Idaho Street. The closest regional freeway is State Route 57, located about five miles east of the project site, but State Highways 39 and 90 are each less than a mile from the project site.

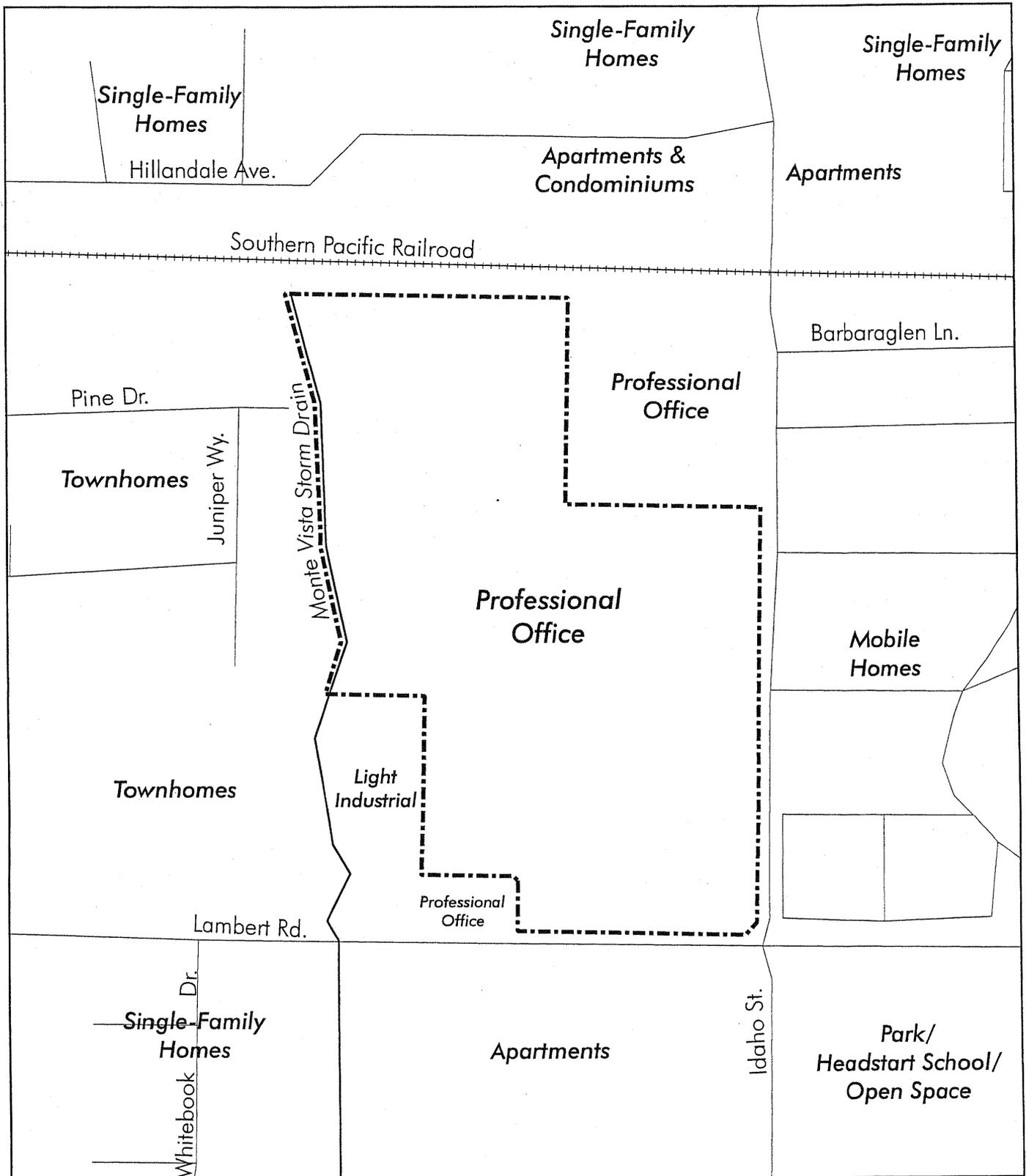
The Orange County Transportation Authority operates a bus route (Route 20) that runs along Idaho Street; its closest stop to the project site is about 0.5 miles south of the Lambert Road/Idaho Street intersection.

The City of La Habra proposes to expand Lambert Road from a four-lane divided roadway to a six-lane divided roadway as part of a citywide road improvement program. The additional lanes are planned in conjunction with a proposal to reduce the width of La Habra Boulevard, the next east-west arterial north of Lambert Road, through La Habra's city center. Average daily traffic on Lambert Road is expected to increase to 34,000 vehicles per day by the year 2025. In a report on this program conducted for the City, the report states that Lambert Road can be expanded to six lanes near the project site through the elimination of existing curbside parking and a bicycle lane.

## SURROUNDING LAND USES

The site is located within a diverse neighborhood consisting of medium-density and high-density residential homes (including a mobile home park), professional office and light manufacturing uses, and a public park. (See Figure 3) Active freight railroad tracks operated by the Southern Pacific Railroad run along the northern site boundary, and across the tracks is a development of single- and multi-family residences. At the northeast corner of the site is a relatively new four-story office building and associated surface parking lot. On Lambert Road southwest of the subject property is a small medical office building. (A separate specific plan is being processed for this site.) Across Lambert Road to the south and southwest are apartments; across Idaho Street to the east is a mobile home park. A fire sprinkler manufacturing business and drainage course bound the southwest corner of the site. Beyond the fire sprinkler business and the drainage course is a multi-family development. A public park is located to the southeast, across Lambert Road and Idaho Street.





Source: William Hezmalhalch Architects, Inc.,  
Cotton/Bridges/Associates, November 2003

 Specific Plan Area



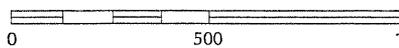
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0 500 1,000

Figure 3  
Existing and Surrounding Land Uses

# DEVELOPMENT SETTING AND CONCEPT

## RELATIONSHIP TO GENERAL PLAN AND ZONING REGULATIONS

The La Habra General Plan designates the site *Professional Office*, which permits professional and medical office uses. The site is zoned Commercial and Professional Office Zone (C-P), with a Planned Unit Development (PUD) overlay. Residential uses are not permitted in the C-P zoning district. As part of the application to approve this Specific Plan, both a General Plan Amendment and a Zone Change will be required. The intensity of use of the proposed Specific Plan area is consistent with the City of La Habra's maximum intensity for *Low Density Residential* uses.

## 2.2 DEVELOPMENT CONCEPT

The Plan proposes a master-planned, gated residential community with supporting active recreation area and open space. Up to 112 detached single-family homes are planned for the 18.6-acre site. The project will include a sidewalk along Idaho Street to link the project to a possible future trail along the adjacent railroad right-of-way. An enhanced landscape feature will be sited in the landscape setback area at the corner of Lambert Road and Idaho Street. The internal circulation system will consist of private roadways. Figure 4 illustrates the conceptual site plan. Land uses are summarized in Table 2-1.

### PLAN CONCEPT

**TABLE 2-1  
LAND USE TABULATION**

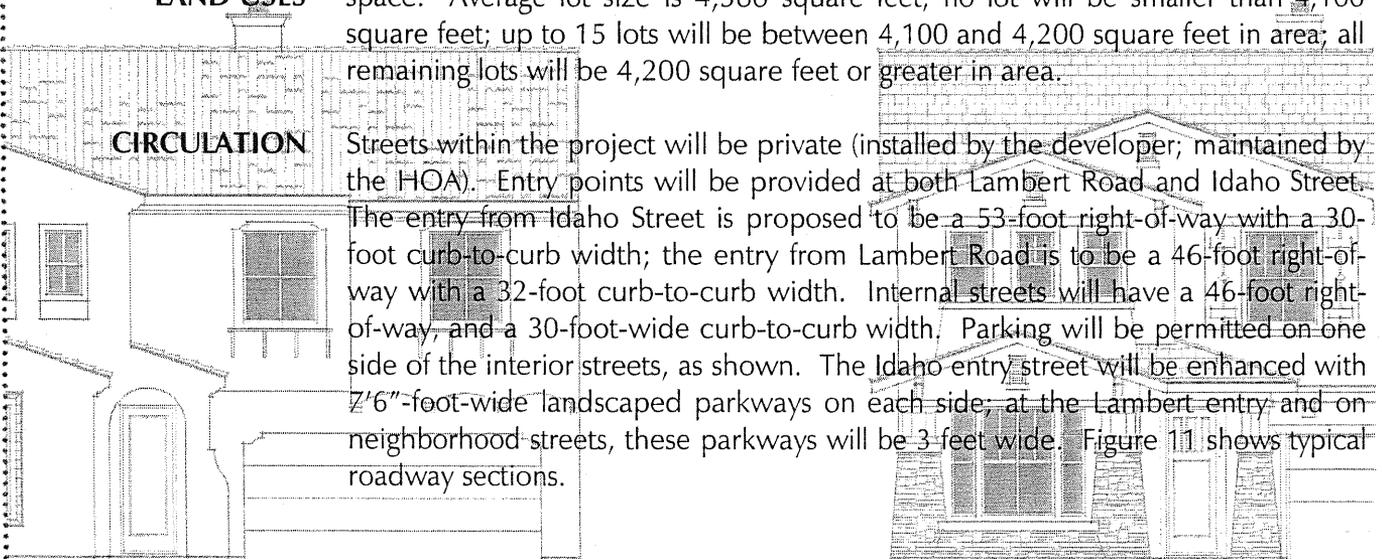
Land Use	Area (ac.)	Dwelling Units
Residential-Single Family Detached	11.77	112
Recreation and Open Space Area	1.25	NA
Other (includes circulation)	5.56	NA
<b>Total</b>	<b>18.6</b>	<b>112</b>

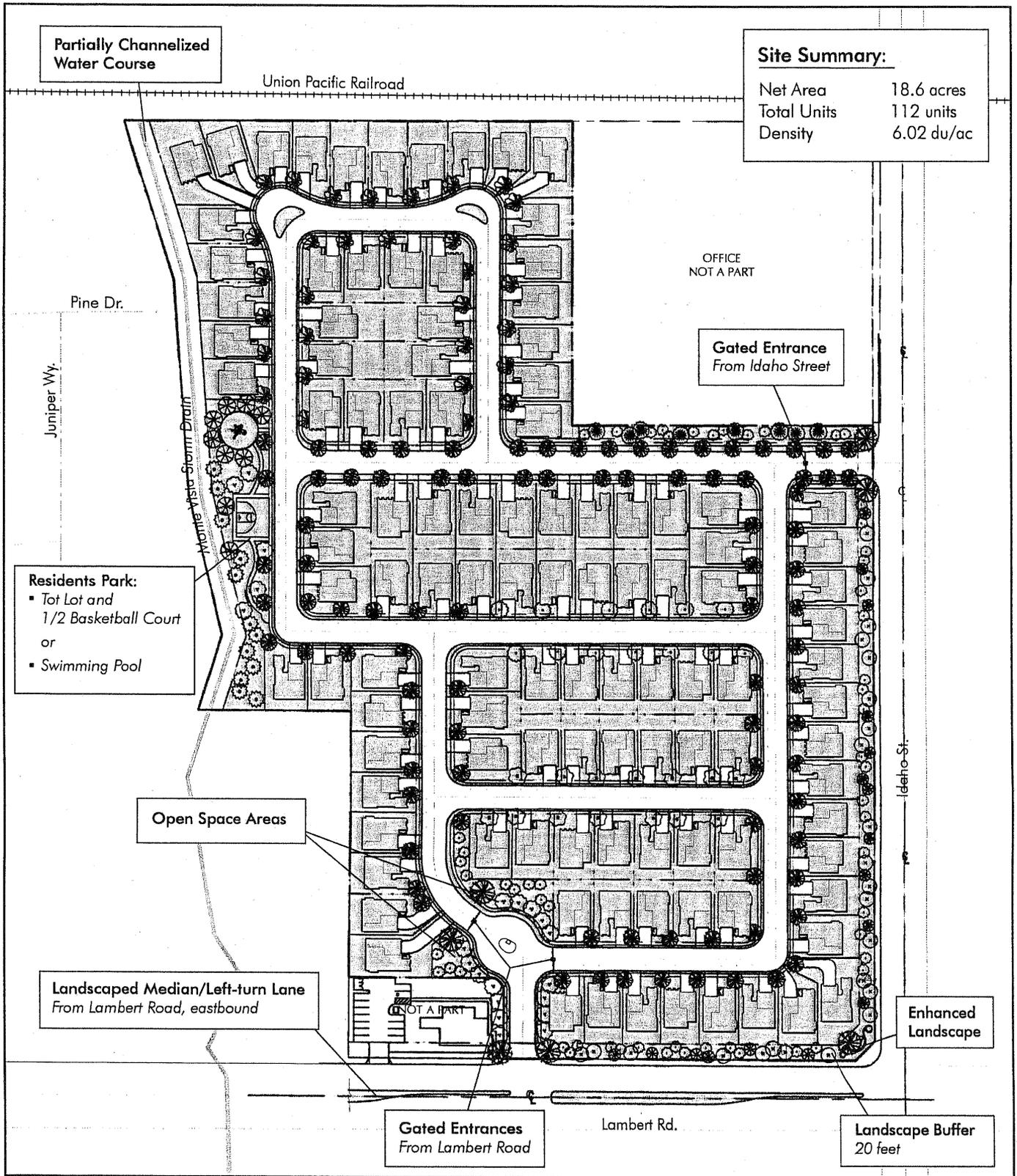
## PERMITTED LAND USES

Permitted uses are restricted to detached single-family homes and park/open space. Average lot size is 4,580 square feet; no lot will be smaller than 4,100 square feet; up to 15 lots will be between 4,100 and 4,200 square feet in area; all remaining lots will be 4,200 square feet or greater in area.

## CIRCULATION

Streets within the project will be private (installed by the developer; maintained by the HOA). Entry points will be provided at both Lambert Road and Idaho Street. The entry from Idaho Street is proposed to be a 53-foot right-of-way with a 30-foot curb-to-curb width; the entry from Lambert Road is to be a 46-foot right-of-way with a 32-foot curb-to-curb width. Internal streets will have a 46-foot right-of-way, and a 30-foot-wide curb-to-curb width. Parking will be permitted on one side of the interior streets, as shown. The Idaho entry street will be enhanced with 7'-6"-foot-wide landscaped parkways on each side; at the Lambert entry and on neighborhood streets, these parkways will be 3 feet wide. Figure 11 shows typical roadway sections.



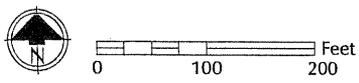


**Site Summary:**

Net Area	18.6 acres
Total Units	112 units
Density	6.02 du/ac

Source: William Hezmalhalch Architects, Inc.,  
Cotton/Bridges/Associates, November 2003

Note:  
Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.



**Figure 4  
Concept Plan**

# DEVELOPMENT SETTING AND CONCEPT

Along both the Lambert Road and Idaho Street frontages, the project will include a 20-foot-wide parkway, a meandering sidewalk, and extensive landscaping. The corner of Lambert Road and Idaho Street will exhibit an enhanced landscaped feature.

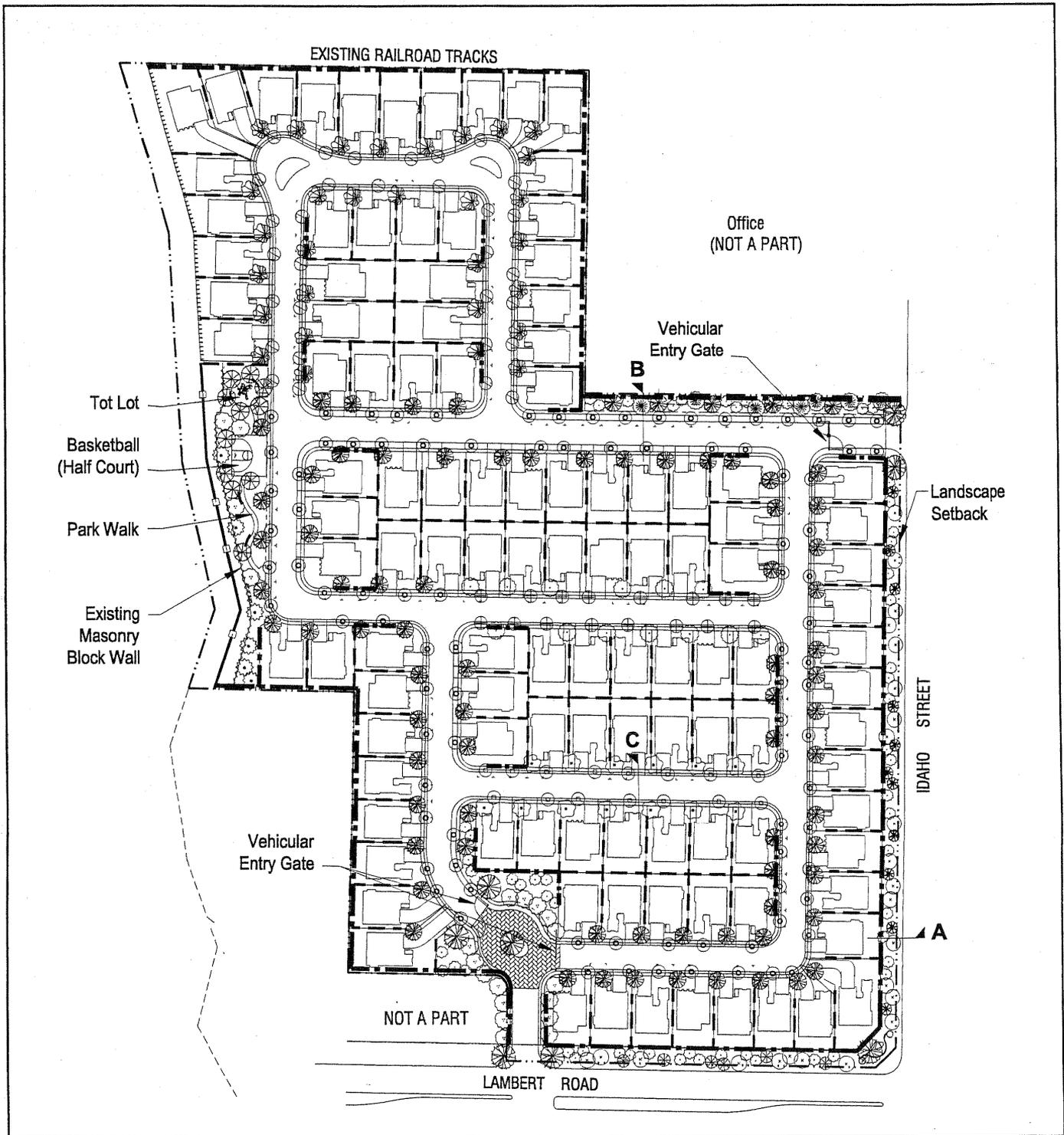
**GRADING** Most of the project site is relatively flat. Two different grading options are under consideration for the site; 100 percent drainage to the channel (Monte Vista Storm Drain) and a roughly equal split of storm drainage to the channel and to Idaho Street. The two conceptual grading and storm drain plans for the site are illustrated on Figures 12a and 12b.

**OPEN SPACE, LANDSCAPING, AND RECREATION PLAN** The recreation plans provide for an active park along the partially channelized water course and landscaped open space areas, including the parkways, neighborhood entry points, and setbacks from the main roads. These landscape, open space, and recreation areas and the details of the two park options are illustrated on Figures 5a1, 5a2, 5b1 and 5b2. None of the trees currently within the project site will be retained; but all efforts will be made to preserve and transition into the project any trees that are planted along Lambert Road and Idaho Street in compliance with the City's "vacant lot" ordinance and in concert with a landscaping plan submitted by the property owner and approved by the City. Any of these trees that cannot be preserved during project construction will be replaced with comparable trees. All new trees will be planted according to the tree legend on Figure 6. Figures 7 and 7a illustrate landscaping cross sections.

The project includes two options for the park area along the channel. The active park amenities of the first plan, which is illustrated on Figures 5a1 and 5a2, include a basketball court and tot lot. The alternative recreation plan includes a swimming pool, a spa and restroom, as shown in Figures 5b1 and 5b2. The neighborhood park will be for use by residents of the development. The two additional landscaped open spaces are located near the Lambert Road gated entrance. Along the perimeter wall that fronts Lambert and Idaho, a 20-foot landscaped buffer runs from the Lambert entrance to the Idaho entrance.

In-lieu of providing on-site affordable housing and public park areas, the project is subject to in-lieu fees. The developer will pay the City in-lieu park fees of \$4,050 per unit and affordable housing fees of approximately \$5,000 per unit.

**UTILITIES PLAN** The Plan includes provisions for utilities, including water, sewer, and drainage improvements to serve the development, and identifies the private utility companies that will be responsible for providing waste disposal, natural gas, electricity, and communications facilities and services. On-site utilities will tie into trunk lines located in Lambert Road and Idaho Street. Drainage systems will be engineered to direct flow into storm drains in the street and/or to the partially channelized drainage course along the western edge of the project site. Either



Source: IMA Design

Note:  
 Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

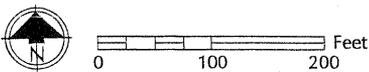
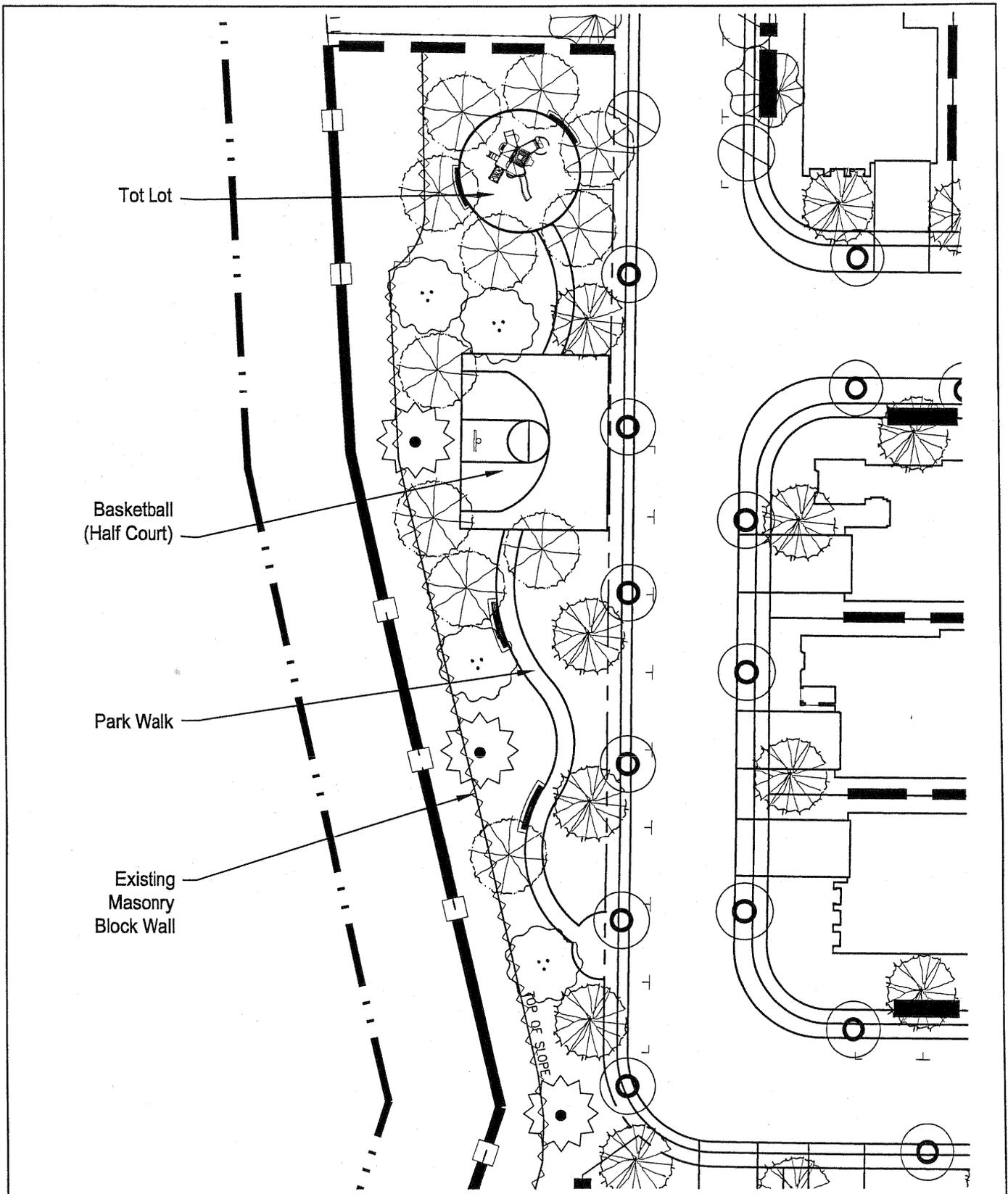


Figure 5a1  
 Open Space, Landscaping,  
 and Recreation Plan-Basketball Court



Source: IMA Design

Note:  
 Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

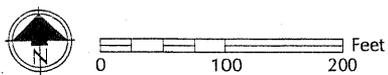
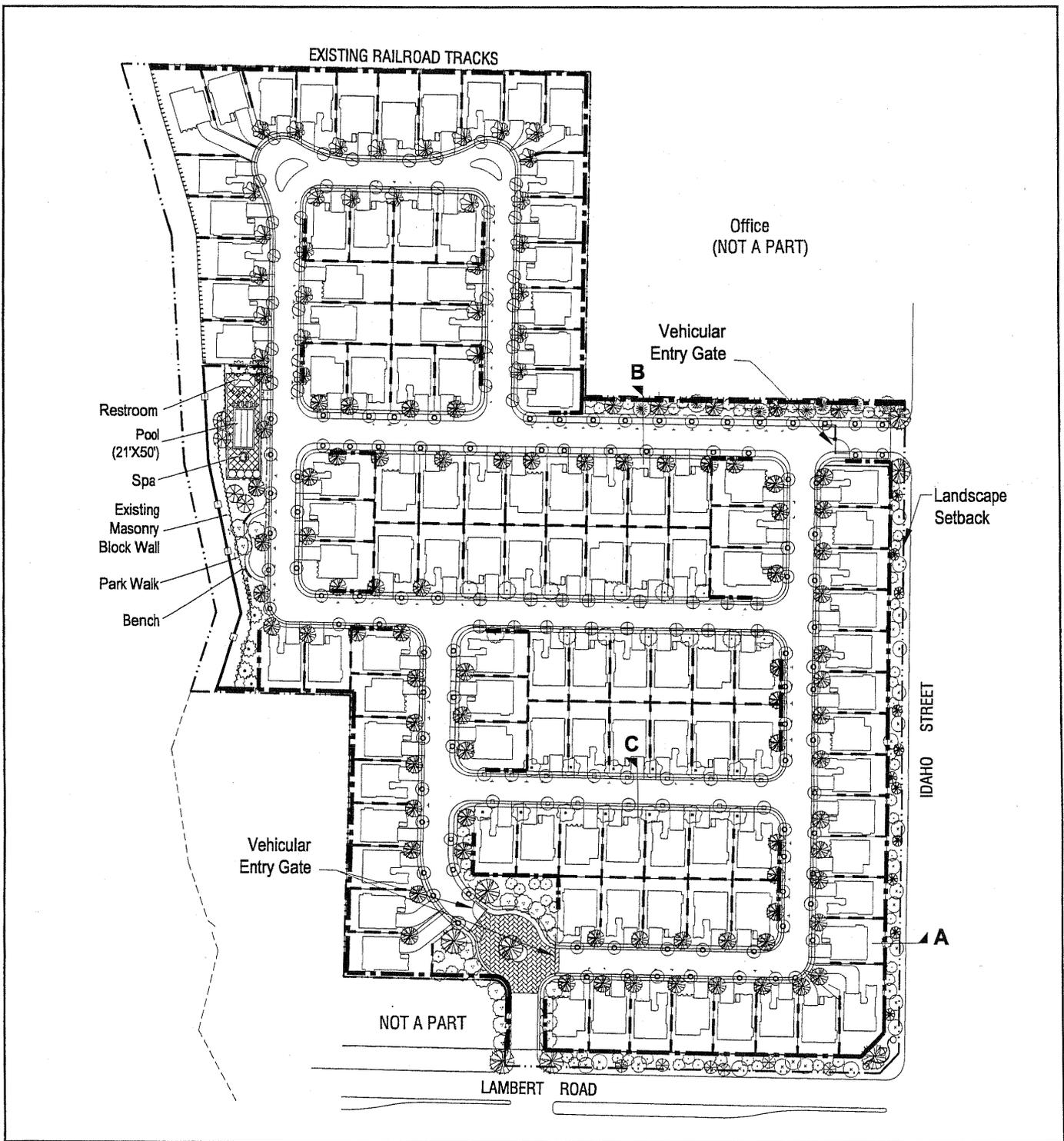


Figure 5a2  
 Basketball Court Enlargement



Source: IMA Design

Note:

Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

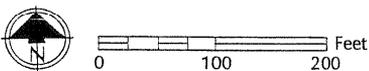
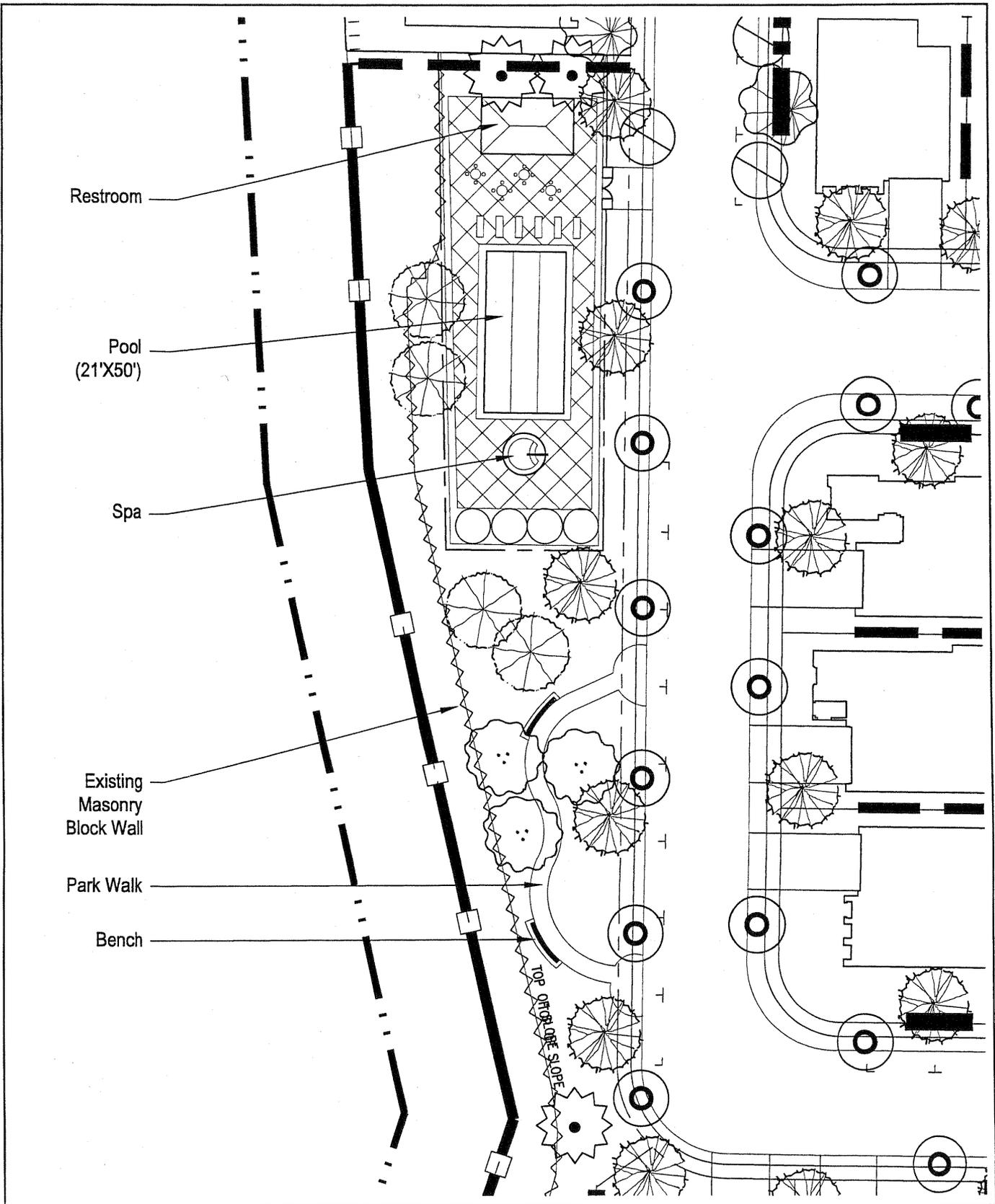


Figure 5b1  
Open Space, Landscaping,  
and Recreation Swimming Pool



Source: IMA Design

Note:  
Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

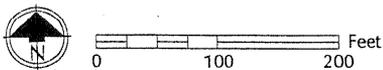


Figure 5b2  
Swimming Pool Enlargement

# TREE LEGEND

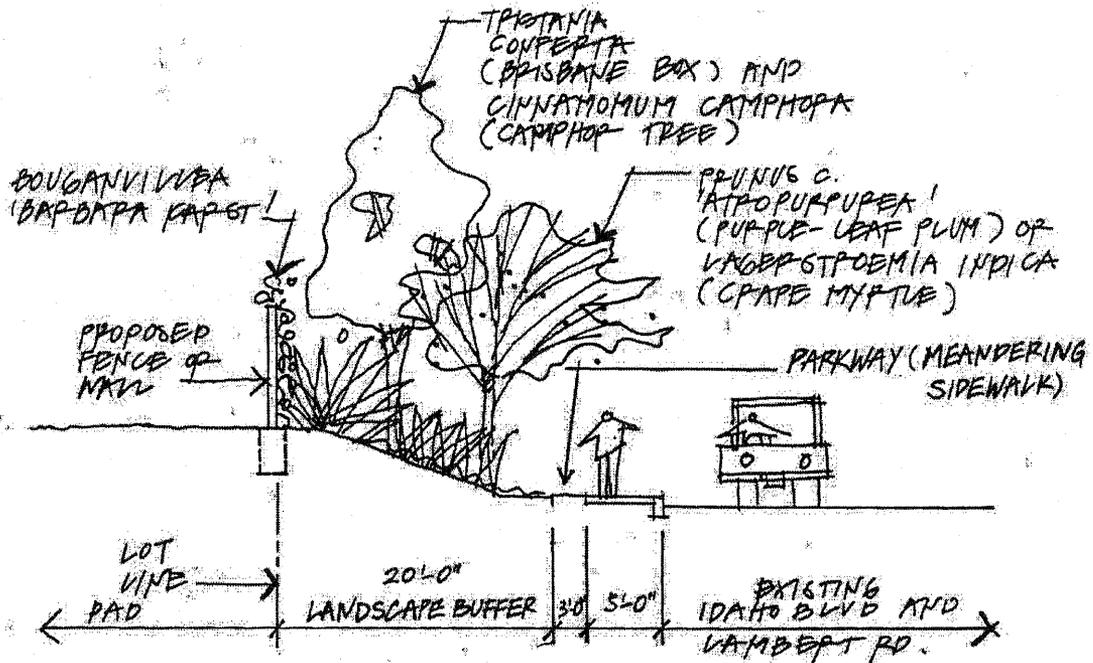
SYMBOLS	BOTANICAL NAME	COMMON NAME	SIZE
<b>Lambert / Idaho Streetscape</b>			
	<i>Cinnamomum camphora</i>	Camphor Tree	36" Box
	<i>Lagerstroemia indica</i>	Crape Myrtle	36" Box
	<i>Prunus c. 'Atropurpurea'</i>	Purple-Leaf Plum	36" Box
	<i>Tristania conferta</i>	Brisbane Box	36" Box
<b>Entrance Landscape</b>			
	<i>Koelreuteria bipinnata</i>	Chinese Flame Tree	36" Box
	<i>Magnolia grandiflora 'St. Mary'</i>	Southern Magnolia	36" Box
	<i>Tristania conferta</i>	Brisbane Box	24" Box
<b>Neighborhood Streetscape</b>			
	<i>Brachychiton populneus</i>	Bottle Tree	24" Box
	<i>Liquidambar styraciflua</i>	American Sweet Gum	24" Box
	<i>Magnolia grandiflora</i>	Southern Magnolia	24" Box
	<i>Olea europaea 'Swan Hill'</i>	Olive	24" Box
	<i>Pistacia chinensis</i>	Chinese Pistache	24" Box
Note: Minimum One Tree for Each Lot			
<b>Parkway Streetscape</b>			
	<i>Bauhinia blakeana</i>	Hong Kong Orchid	15 Gal.
	<i>Chitalpa tashkentensis</i>	Chitalpa	15 Gal.
	<i>Eriobotrya deflexa</i>	Bronze Loquat	15 Gal.
	<i>Lagerstroemia indica 'Tuscarora'</i>	Crape Myrtle	15 Gal.
Note: Minimum One Tree for Each Lot			
<b>Park Landscape</b>			
	<i>Platanus racemosa</i>	California Sycamore	24" Box
	<i>Pinus eldarica</i>	Afgan Pine	24" Box

Source: IMA Design

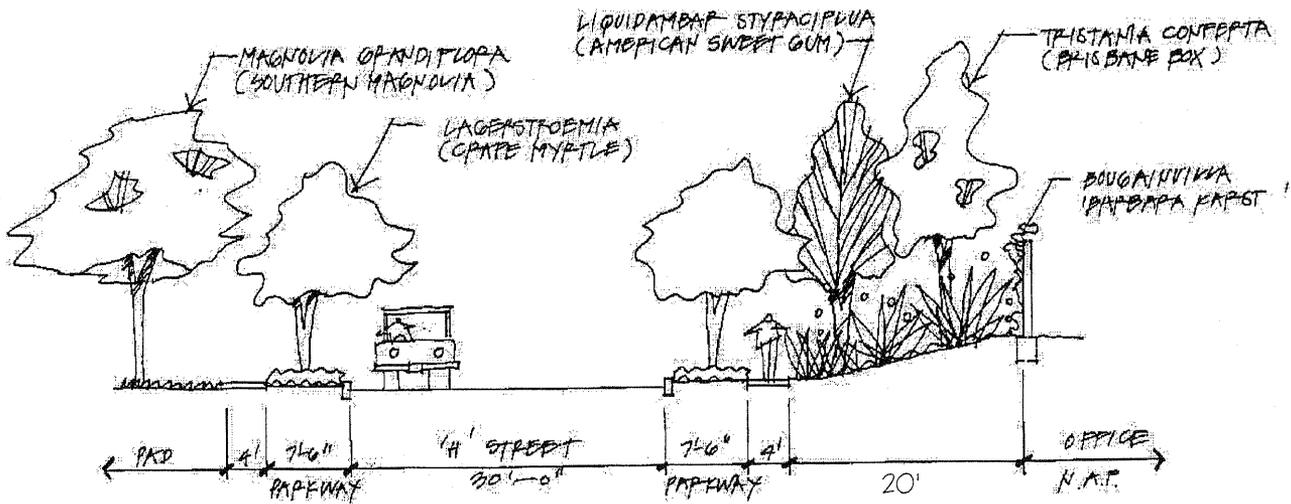
Note:

Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

Figure 6  
Tree Legend



Cross Section from Property Line to Lambert Road/Idaho Street



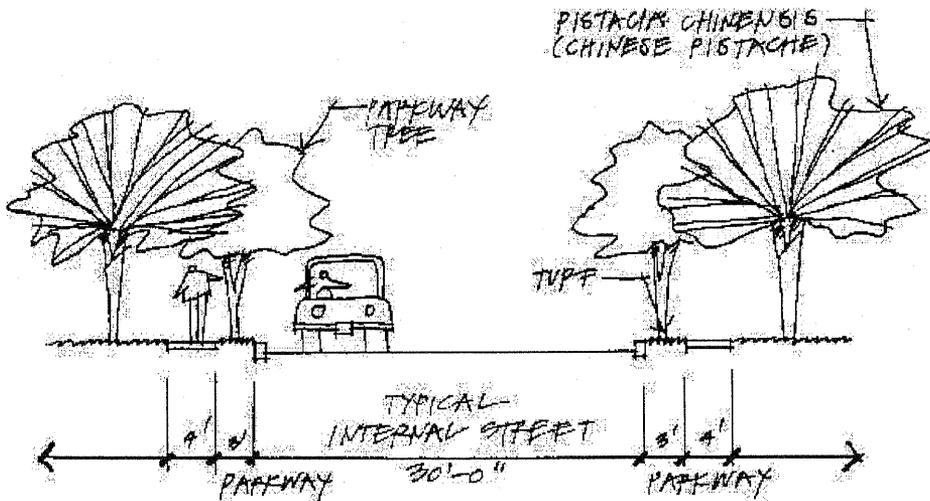
Cross Section Looking West on "H" Street, North of Lots 43/42

Source: IMA Design



Not to Scale

Figure 7  
Landscape Cross Sections



Cross Section Through  
Typical Internal Street

(Not Shown: On-street parking allowed only  
on one side of street at any given point)

Source: IMA Design



Not to Scale

Figure 7a  
Landscape Cross Sections

# DEVELOPMENT SETTING AND CONCEPT

drainage scheme will ensure that run-off quality meets Regional Water Quality Control Board standards. Chapter 4 discusses all infrastructure in greater detail.

In the project vicinity, the partially channelized drainage course, also known as the Monte Vista Storm Drain, (which flows into Coyote Creek south of Lambert Road and the project site) exists as an above-ground, concrete-lined channel north of Lambert Road and as an underground pipe (beneath the railroad tracks) north of the site. At the site, the channel consists of an unimproved, vegetated wash.

**PROJECT PHASING** The project will be built in a single phase, including the open space improvements. A two-year development schedule is anticipated.





# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

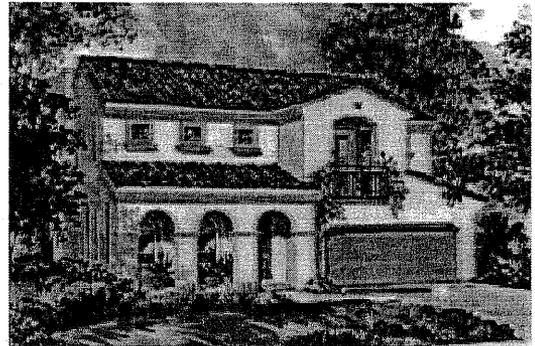
This chapter defines land use regulations and establishes development standards and design criteria for their proper execution. These standards and guidelines provide criteria to guide residents, property owners, and the City of La Habra in the physical development of the Plan area.

## 3.1 LAND USE REGULATIONS

The Specific Plan provides for detached single-family residential uses that correspond to the *Low Density Residential* land use designation in the La Habra General Plan. Specific Plan land use regulations and development standards will serve as zoning regulations specific to this site.

### *Low Density Residential – Detached Single-Family*

This category provides for the development of dwelling units that incorporate front, back, and side yards into individual living environments each containing street frontage onto a private street that will engage pedestrians. The category allows for a density of 6.02 dwelling units per gross acre (9.5 dwelling units per acre net). All uses other than residential detached single-family homes are prohibited.



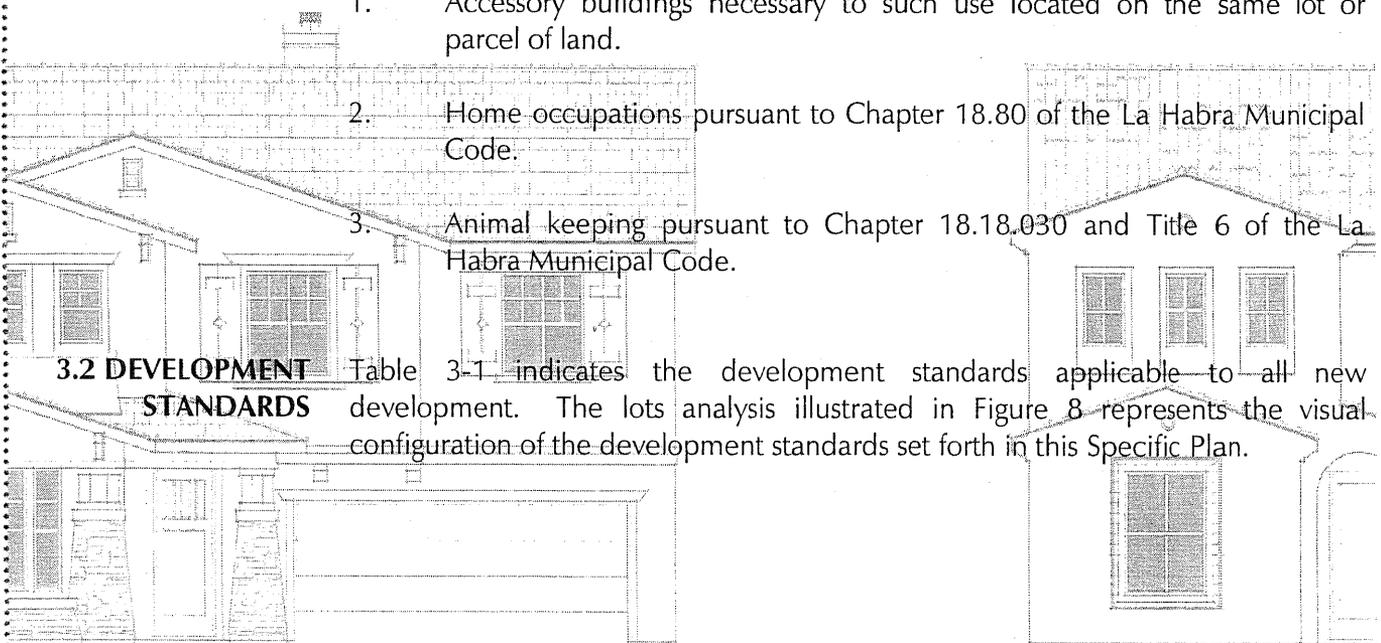
### *Accessory Uses*

The following accessory uses are permitted:

1. Accessory buildings necessary to such use located on the same lot or parcel of land.
2. Home occupations pursuant to Chapter 18.80 of the La Habra Municipal Code.
3. Animal keeping pursuant to Chapter 18.18.030 and Title 6 of the La Habra Municipal Code.

## 3.2 DEVELOPMENT STANDARDS

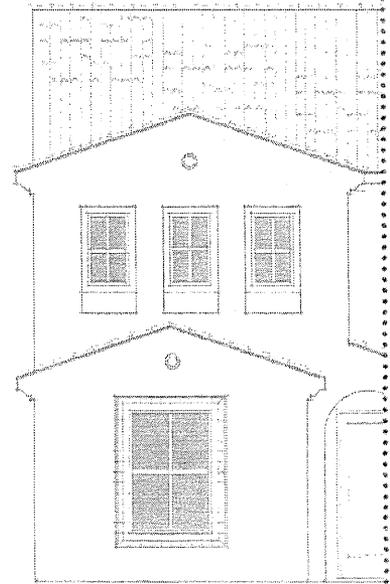
Table 3-1 indicates the development standards applicable to all new development. The lots analysis illustrated in Figure 8 represents the visual configuration of the development standards set forth in this Specific Plan.

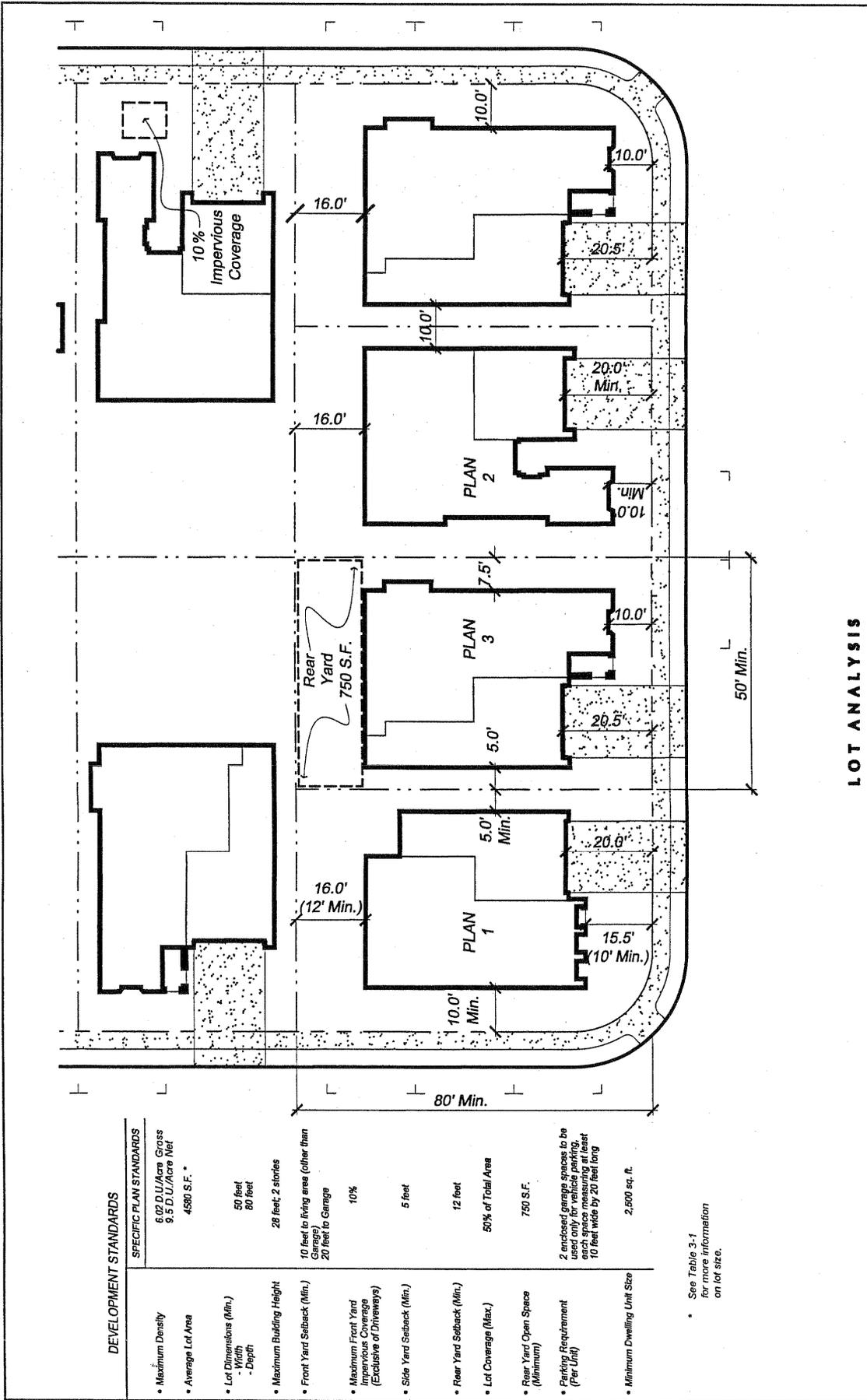


# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

**TABLE 3-1  
DEVELOPMENT STANDARDS**

Development Standard	Specific Plan Standards
1. Maximum Density	6.02 du/acre net (approx. 9.5 du/ac gross)
2. Average Lot Area	4,580 sq ft 98 lots will be greater than 4,200 sq. ft. 15 lots will measure between 4,100 sq. ft. and 4,100 sq. ft. No lots smaller than 4,100 sq. ft.
3. Lot Dimensions (min) <ul style="list-style-type: none"> <li>▪ Width</li> <li>▪ Depth</li> </ul>	50 feet 80 feet
4. Maximum Building Height	28 feet; 2 stories
5. Front Yard Setback (Minimum)	10 feet to living area (other than garage) 20 feet to garage
5a. Maximum front yard impervious coverage (exclusive of driveways)	10%
6. Side Yard Setback (Minimum)	5 feet
7. Rear Yard Setback (Minimum)	12 feet
8. Lot Coverage (Maximum)	50% of total lot area
9. Rear yard open space (Minimum requirement)	750 sq. ft.
10. Parking requirement (Per unit)	2 enclosed garage spaces to be used only for vehicle parking, each space measuring at least 10 feet wide by 20 feet long
11. Minimum dwelling unit size	2,500 square feet





**LOT ANALYSIS**

**DEVELOPMENT STANDARDS**

SPECIFIC PLAN STANDARDS	
• Maximum Density	6.02 D.U./Acres Gross 9.5 D.U./Acres Net
• Average Lot Area	4580 S.F. *
• Lot Dimensions (Min.) - Width - Depth	50 feet 80 feet
• Maximum Building Height	28 feet, 2 stories
• Front Yard Setback (Min.)	10 feet to living area (other than Garage) 20 feet to Garage
• Maximum Front Yard Impervious Coverage (Exclusive of Driveways)	10%
• Side Yard Setback (Min.)	5 feet
• Rear Yard Setback (Min.)	12 feet
• Lot Coverage (Max.)	50% of Total Area
• Rear Yard Open Space (Minimum)	750 S.F.
• Parking Requirement (Per Unit)	2 enclosed garage spaces to be used only for vehicle parking, each space measuring at least 10 feet wide by 20 feet long
• Minimum Dwelling Unit Size	2,500 sq. ft.

\* See Table 3-1  
for more information  
on lot size.

Source: William Hezmalhalch Architects Inc.,  
Cotton/Bridges/Associates, November 2003

Note:  
Site Plan is subject to minor modifications addressing internal circulation and  
ingress/egress resulting from recommendations of the project traffic study.

**Figure 8**  
**Typical Site Plan**

# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

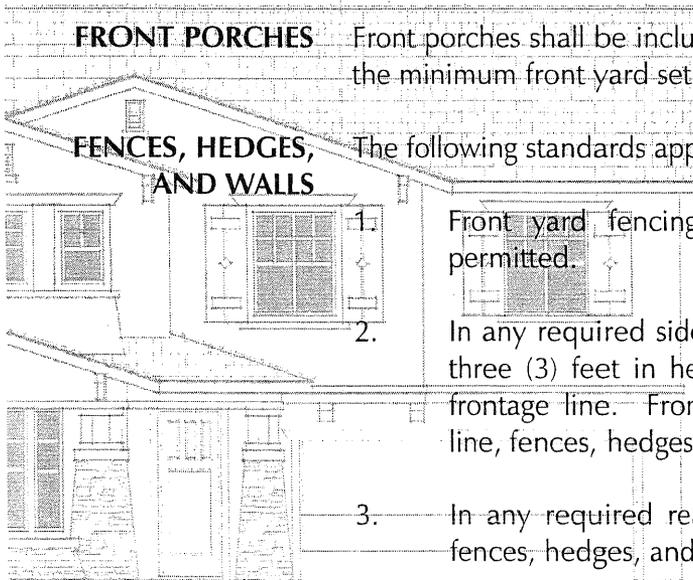
## PERMITTED PROJECTIONS

Architectural components that extend beyond the building face create interest and variability in a project's design. Front porches can be an integral architectural element in a residential development. To provide for these elements, the Plan permits architectural and structural features to project into required setbacks as set forth in Table 3-2. No side yard projections are permitted, except for fireplace structures, as noted in Table 3-2. Projections not specifically listed below are not permitted.

A patio roof/shade structure shall project no more than ten (10) feet into any required rear yard area, and in no case shall the rear yard setback be less than five (5) feet. In no case shall more than fifty (50) percent of the required rear yard area be utilized for a patio roof/shade structure. A patio roof/shade structure is not a patio enclosure.

**TABLE 3-2  
PERMITTED PROJECTIONS**

Permitted Projection Type	Permitted Location(s)	Permitted Projection Distance
Eaves, cornices, bay windows, other architectural features	Rear yard	3 feet
Fireplace structures, 8 feet wide or less	Rear and side yards	3 feet maximum on rear yard, 2 feet for side yard
Balconies	Rear yards	3 feet maximum
Guard railings around ramps in the front yard	Front yard	3½ feet maximum height
Chimneys	At rooftops	May exceed building height limit by no more than 4 feet



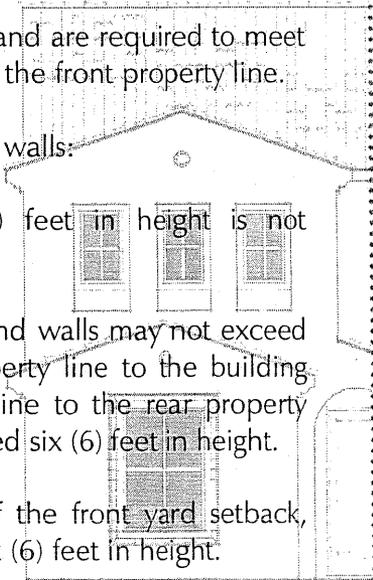
### FRONT PORCHES

Front porches shall be included in the building area and are required to meet the minimum front yard setback of ten (10) feet from the front property line.

### FENCES, HEDGES, AND WALLS

The following standards apply to fences, hedges, and walls:

1. Front yard fencing greater than three (3) feet in height is not permitted.
2. In any required side yard, fences, hedges, and walls may not exceed three (3) feet in height from the front property line to the building frontage line. From the building frontage line to the rear property line, fences, hedges, and walls may not exceed six (6) feet in height.
3. In any required rear yard or at the rear of the front yard setback, fences, hedges, and walls shall not exceed six (6) feet in height.

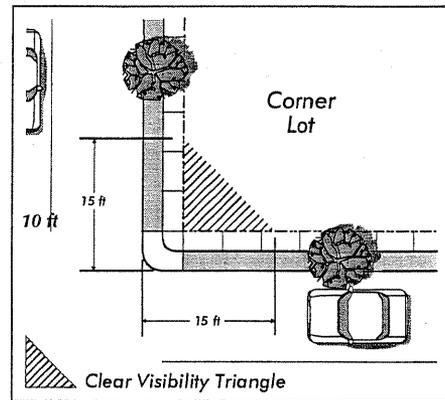


# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

4. For those properties whose rear or side yards are adjacent to the proposed specific plan boundary or along public or private street frontage, a solid masonry wall shall be required.

5. All fence and wall design shall comply with design criteria contained in subsequent sections of this Plan.

6. Acceptable fencing materials for residential lots shall consist of masonry and/or natural wood except along the project's western edge. Figure 9 illustrates all acceptable fence and wall materials and their locations within the project.



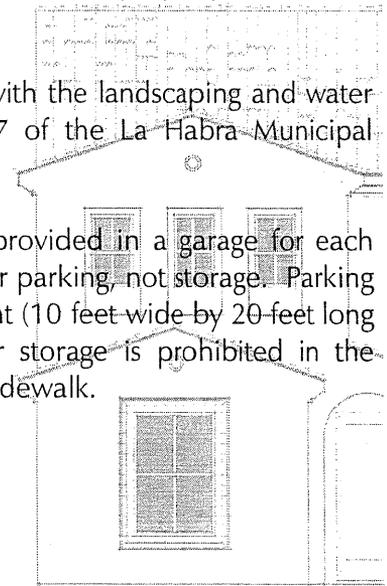
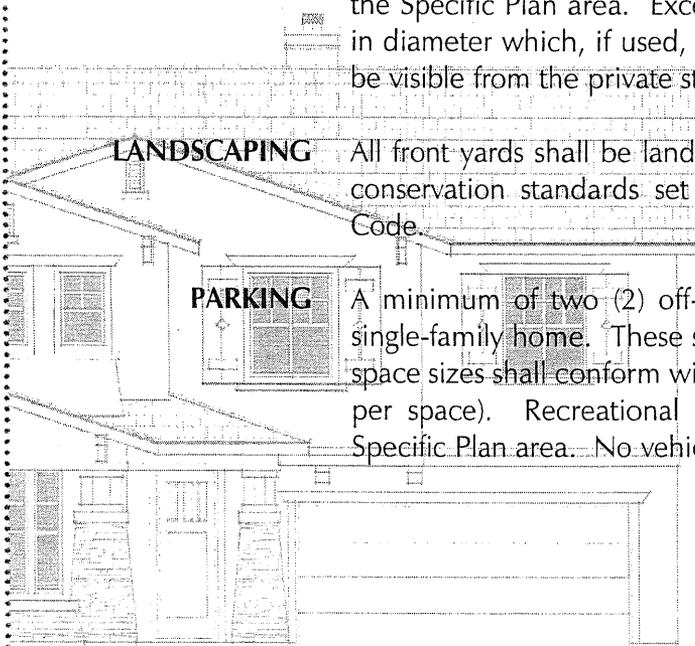
*A clear cross-visibility area refers to an intersection unobstructed by structures, fences/walls or landscaping. For corner lots, fences/walls and hedges are prohibited ten (10) feet from the corner in either direction to maintain visibility.*

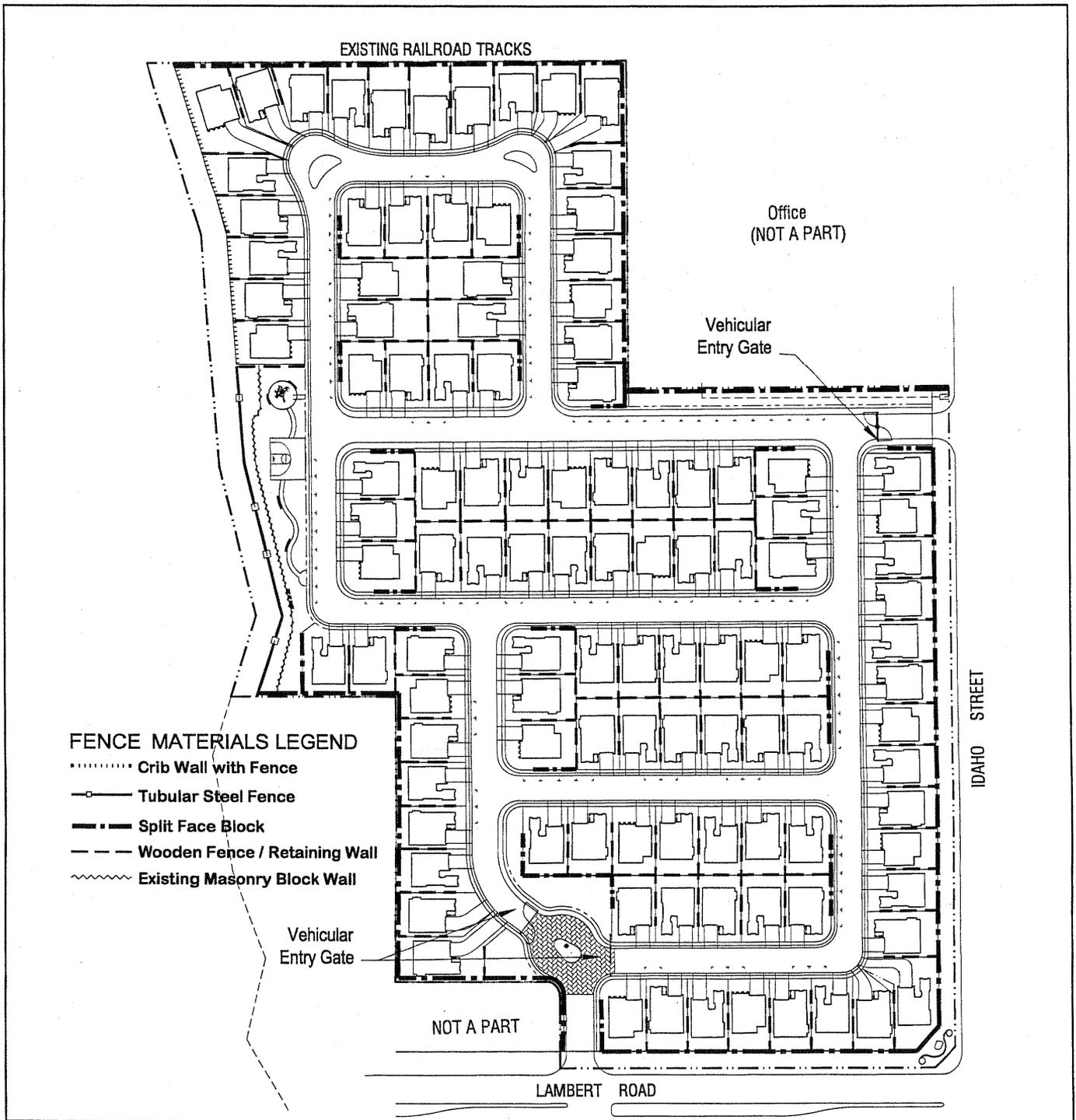
**PUBLIC UTILITIES** All on-site utilities including, but not limited to, electrical, water, sewage, and telephone, shall be installed underground.

**ROOF-MOUNTED EQUIPMENT** Roof-mounted mechanical equipment, tanks, ventilation fans, wireless communication facilities, and similar equipment are prohibited on all properties in the Specific Plan area. Exceptions shall include dish antenna less than six (6) feet in diameter which, if used, shall be placed at the rear of the property so as not to be visible from the private street.

**LANDSCAPING** All front yards shall be landscaped in accordance with the landscaping and water conservation standards set forth in Chapter 18.57 of the La Habra Municipal Code.

**PARKING** A minimum of two (2) off-street spaces shall be provided in a garage for each single-family home. These spaces are to be used for parking, not storage. Parking space sizes shall conform with the City's requirement (10 feet wide by 20 feet long per space). Recreational vehicle (RV) parking or storage is prohibited in the Specific Plan area. No vehicles may overhang the sidewalk.





Source: IMA Design

Note:  
 Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

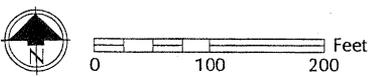


Figure 9  
 Fence Diagram

# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## 3.3 DESIGN GUIDELINES

The architectural, site planning, and landscape architectural guidelines for development within the Specific Plan area are intended to reinforce the development standards and to provide detail regarding the character of development and aesthetic of the neighborhood. Refer to Section 6-4 of this document for a discussion of the Design Review approval process.

## ARCHITECTURAL THEMES

Recognizable and appropriate architectural styles create a sense of place and add visual interest to residential neighborhoods and commercial districts. This Specific Plan identifies a harmonious blend of varied architectural styles that can be used throughout the development. This listing of styles is intended to encourage design that promotes an appreciation of traditional architectural elements. The following styles that may be used in the development include:

**Craftsman** - This style of architecture originated in California during the Arts and Crafts Movement, starting from the late 1890s through the 1920s. Typical features include:

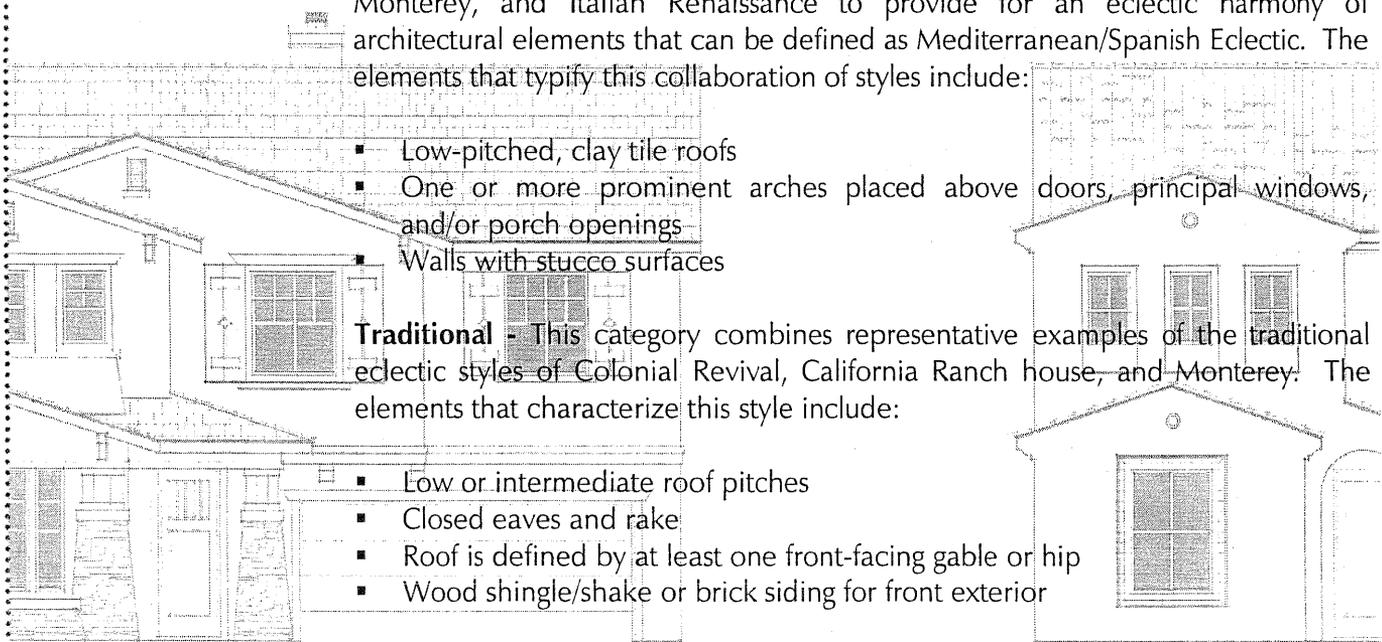
- A boxy or square appearance
- Low-pitched gable roofs with wide, unenclosed eave overhangs and exposed rafters
- Decorative (false) beams or braces added under the gables
- Large front porches supported by tapered, square wood columns adorned with exposed brick or river rock, leading to an exposed foundation made of the same material.

**Mediterranean/Spanish Eclectic** - This broad category combines similar yet distinctive architectural styles, including Spanish Colonial Revival, Mission, Monterey, and Italian Renaissance to provide for an eclectic harmony of architectural elements that can be defined as Mediterranean/Spanish Eclectic. The elements that typify this collaboration of styles include:

- Low-pitched, clay tile roofs
- One or more prominent arches placed above doors, principal windows, and/or porch openings
- Walls with stucco surfaces

**Traditional** - This category combines representative examples of the traditional eclectic styles of Colonial Revival, California Ranch house, and Monterey. The elements that characterize this style include:

- Low or intermediate roof pitches
- Closed eaves and rake
- Roof is defined by at least one front-facing gable or hip
- Wood shingle/shake or brick siding for front exterior

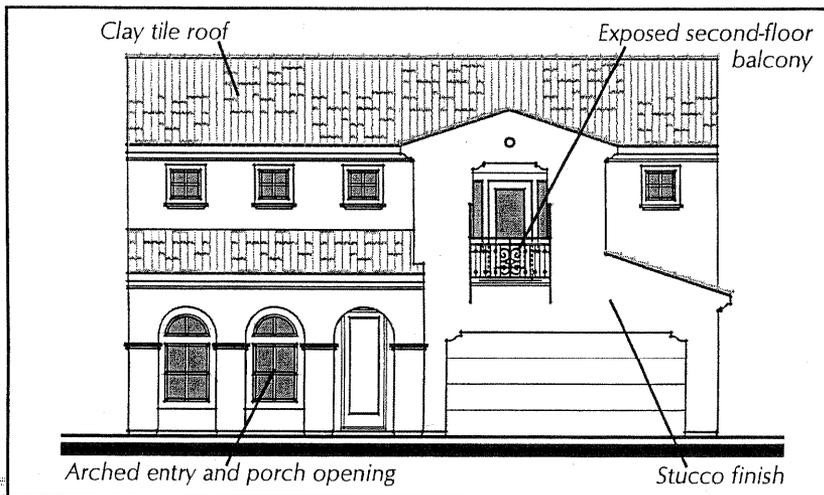
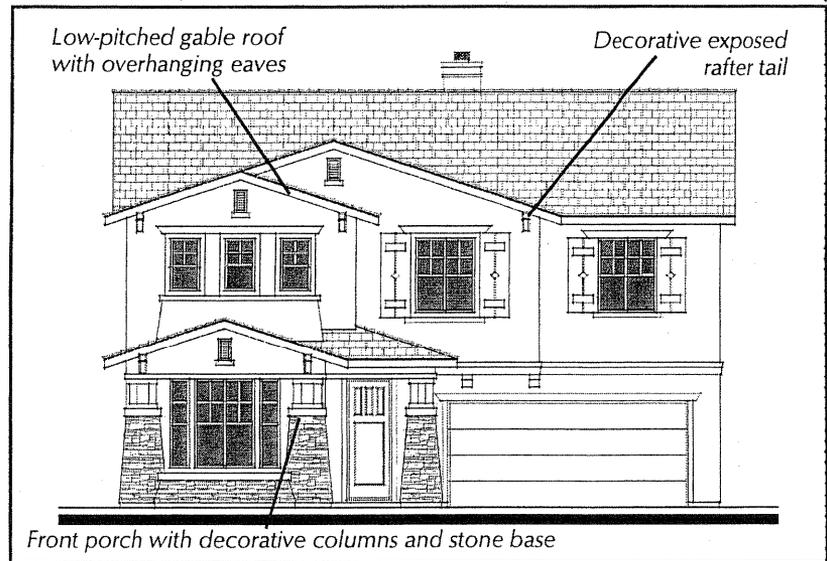


# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

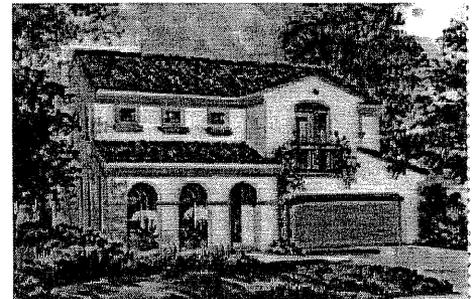
## Craftsman



Elevation\* and perspective rendering\* of a home with Craftsman architectural elements



## Mediterranean/Spanish Eclectic

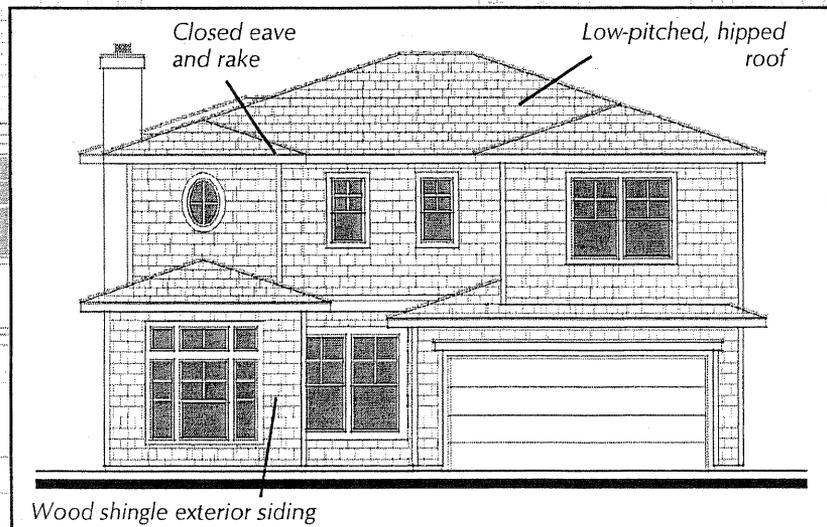


Elevation\* and perspective rendering\* of a home with Spanish Eclectic architectural elements

## Traditional



Elevation\* and perspective rendering\* of a home with Traditional architectural elements



\* Elevations and perspective renderings by Bassenian and Lagoni Architects

# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## SITE DESIGN *Compatibility*

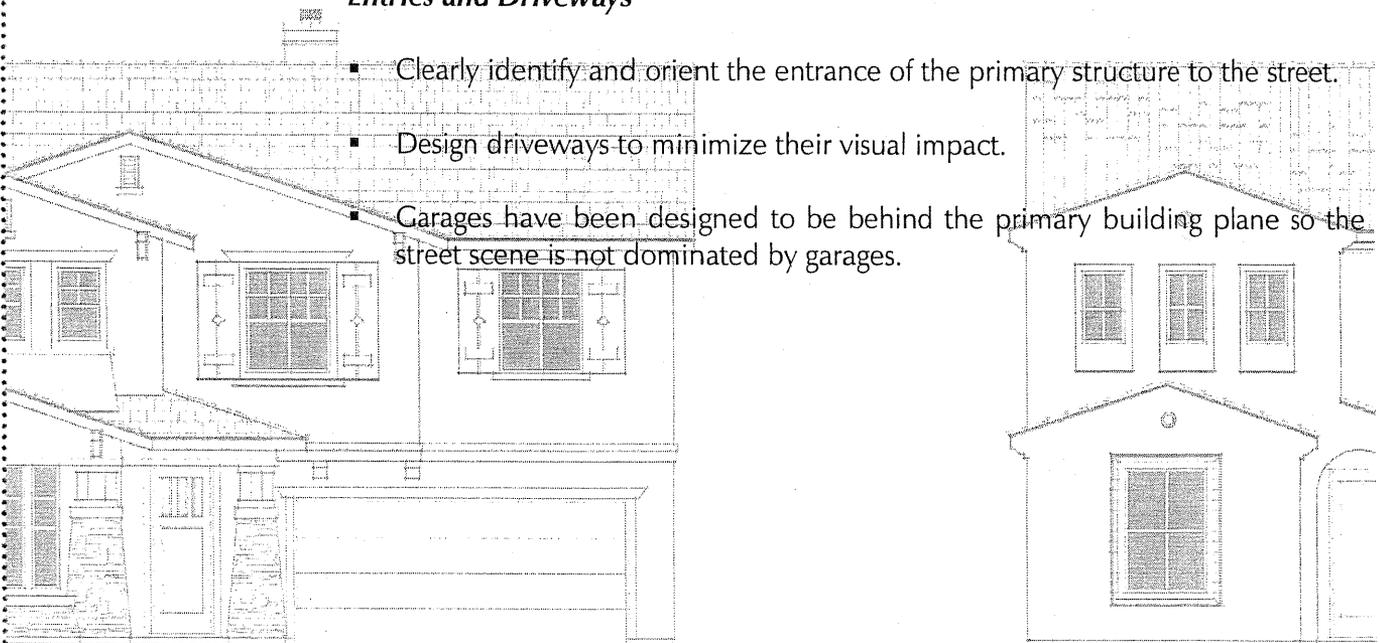
- Compatible architectural styles should be employed within the residential area. These styles should retain their distinct and individual architectural character while creating a harmonious community.
- Building additions to existing structures should be of the same architectural style and design as the primary structure.
- Accessory structures should be compatible in architectural style and design to that of the main structure.



*Architectural details appropriate to the project's architectural style, such as column ornamentation, window casement, and decorative rafter tails for the Craftsman style, provide visual interest and show compatibility.*

## *Entries and Driveways*

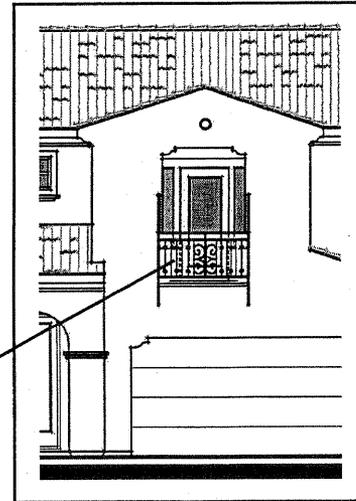
- Clearly identify and orient the entrance of the primary structure to the street.
- Design driveways to minimize their visual impact.
- Garages have been designed to be behind the primary building plane so the street scene is not dominated by garages.



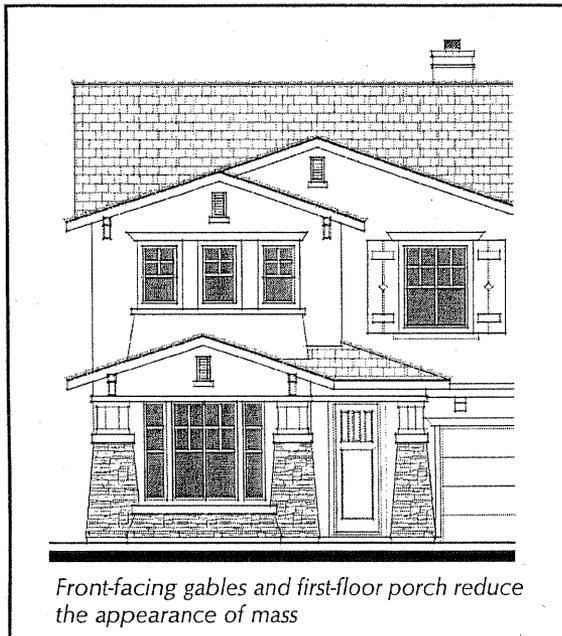
# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## ARCHITECTURAL ELEMENTS *Building Mass*

- Incorporate building projections to enhance façade depth and to provide visual interest.
- Utilize elements such as roof planes, balconies, covered porches, chimneys, windows, and bay windows to reduce the appearance of mass.



*Second-floor balcony provides articulation and visual interest*



*Front-facing gables and first-floor porch reduce the appearance of mass*

### *Front Porches*

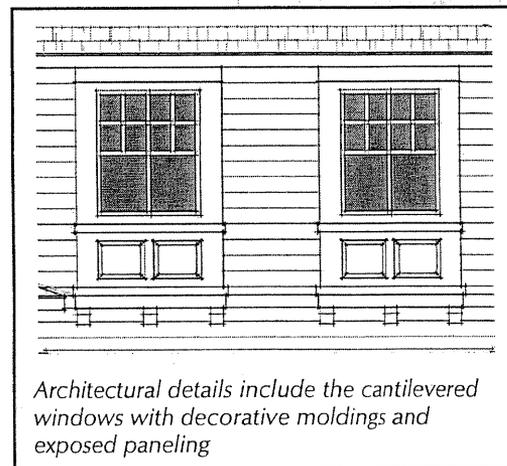
- One-story roofs and overhangs that serve as porches, appropriate to the architectural style, are encouraged.

### *Roof Lines, Angles and Projections*

- Roof lines, roof angles, and projections — including porch roofs and awnings — should be compatible with adjacent structures. Major roof slopes should not fall parallel to the street and should be broken up with front-facing hip and gable framing and pitched dormers to add visual interest.

### *Architectural Details*

- Architectural details including porch columns, decorative door and window designs, roof overhangs with decorative brackets and rafter tails, exterior moldings, porch and balcony railings, exterior siding and lighting fixtures consistent with the project's architectural style provide visual interest and are strongly encouraged.

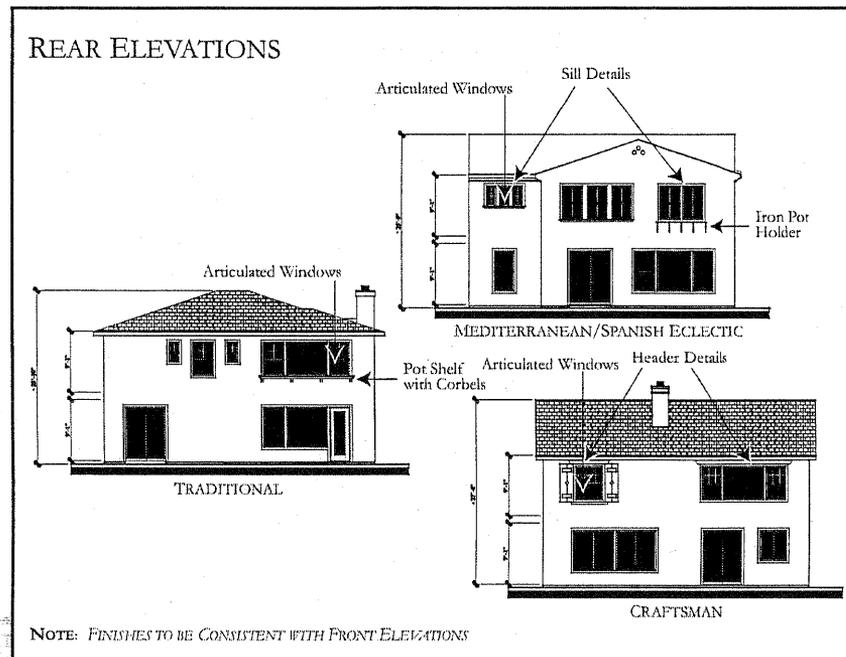


*Architectural details include the cantilevered windows with decorative moldings and exposed paneling*

# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## *Façade Treatment*

- Façade treatment should be consistent for all building planes of the structure. Architectural details consistent with the structure's prevalent architectural style should be incorporated on all façades visible from a public right of way.
- Front and rear façades will be fenestrated with windows and doors to provide visual relief from the appearance of mass and large expanses of blank wall surface. Windows and doors should be consistent in shape, style, and size to each other. The diagram below illustrates the rear elevations for the three design styles; each features recessed windows, many with sills, sashes, and lintels.



- Façades will be articulated to provide visual relief from the appearance of mass and large expanses of blank wall surface. Permitted projections such as bay windows and cantilevered, open and enclosed balconies provide visual interest and relief to the façade.

## *Accessory Lighting*

- Exterior lighting must be positioned so that no direct light extends onto neighboring properties. Illumination must also be screened from adjacent properties. House numbers are to be illuminated per City of La Habra Fire Department standards.

# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## LANDSCAPING *Landscape Characteristics*

- The design of landscape elements pertaining to form, horizontal and vertical lines, hardscape and softscape, and ornate qualities of front yard and common areas visible from the public right-of-way should be compatible with the primary structure. Visual openness should be maintained in front yards and common areas for residential lots.

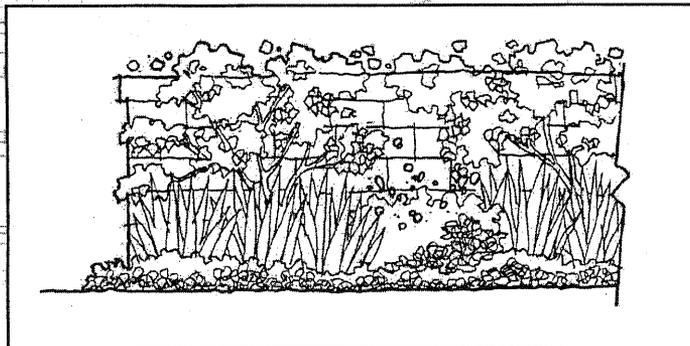


- Landscaping should include one street tree on every lot and another in the right of way of each property.
- Maximize vegetative ground cover that will absorb rainwater and reduce runoff. Use permeable surfaces whenever possible to reduce paving.
- Where appropriate, landscaping should incorporate compatible species of drought-tolerant plants to reduce water demand.
- Mechanical equipment and trash enclosures should be placed out of view from the public right-of-way and should be screened with landscaping.

## *Building Surfaces, Fences, and Walls*

- Blank building surfaces, fences, and walls should incorporate landscaping to screen large masses of blank surface area, particularly where such features are visible from the public right-of-way.

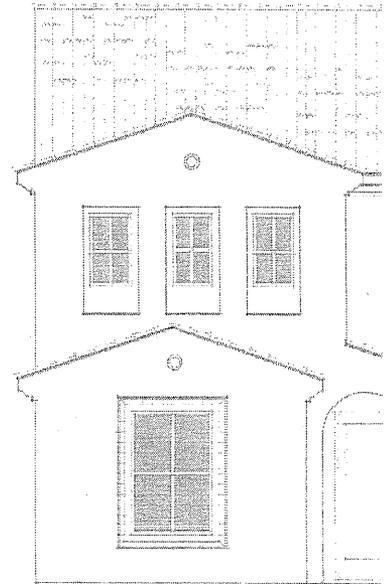
*Blank wall-surface area is appropriately screened with clinging vines and additional ground landscaping*



# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## FENCES AND WALLS

- Fences and walls shall be architecturally compatible with the primary structure and are encouraged to contain design elements, such as decorative cornice, pilasters, and capping, representative of the primary structures' architectural styles.
- Landscaping should be included as part of the design for the fence or wall and should be used to soften and screen large masses of blank surface area.
- Fences between residential properties are required to be made of natural wood (Figure 9 illustrates acceptable fencing materials throughout the Specific Plan area).
- Solid masonry walls will be composed of split-faced block and will be screened by landscaping.





# INFRASTRUCTURE

## 4.1 CIRCULATION IMPROVEMENTS

Within the Specific Plan project site, circulation improvements will include the relocation and reconstruction of driveways to the project site and the creation of the internal private street and landscaped parkway network. These improvements are illustrated on Figure 10.

### ON-SITE CIRCULATION IMPROVEMENTS

Entry driveways will be relocated and/or widened on both Lambert Road and Idaho Street. The widening of these driveways to 32 and 30 feet will improve the safety of vehicles entering the project site from either road. Both driveways will feature gates and vehicle stacking areas. The main site entrance will be on Lambert Road, with secondary access from the Idaho Street driveway.

The Lambert Road entrance will include two gated entries with vehicle stacking areas, the location of which are shown in Figure 4. The placement of these gates will create a sense of security within the community at the project's main entrance. The Idaho entrance will include one remote-controlled gate into the project at the northeast corner of the site.

All internal streets will be private and constructed as per the Specific Plan. As illustrated in Figure 11, entry street right-of-way widths will range from 47 feet to 53 feet, with a paved curb-to-curb width of 30 feet. Neighborhood streets will have a 44-foot right-of-way with a 30-foot curb-to-curb width. Street trees will be provided as per the conceptual landscape plan. Street lights will be Southern California Edison's "Nostalgic."

All internal private streets will have landscaped parkways, which include 4-foot-wide property-line sidewalks and 3-foot minimum planting strips along curblines.

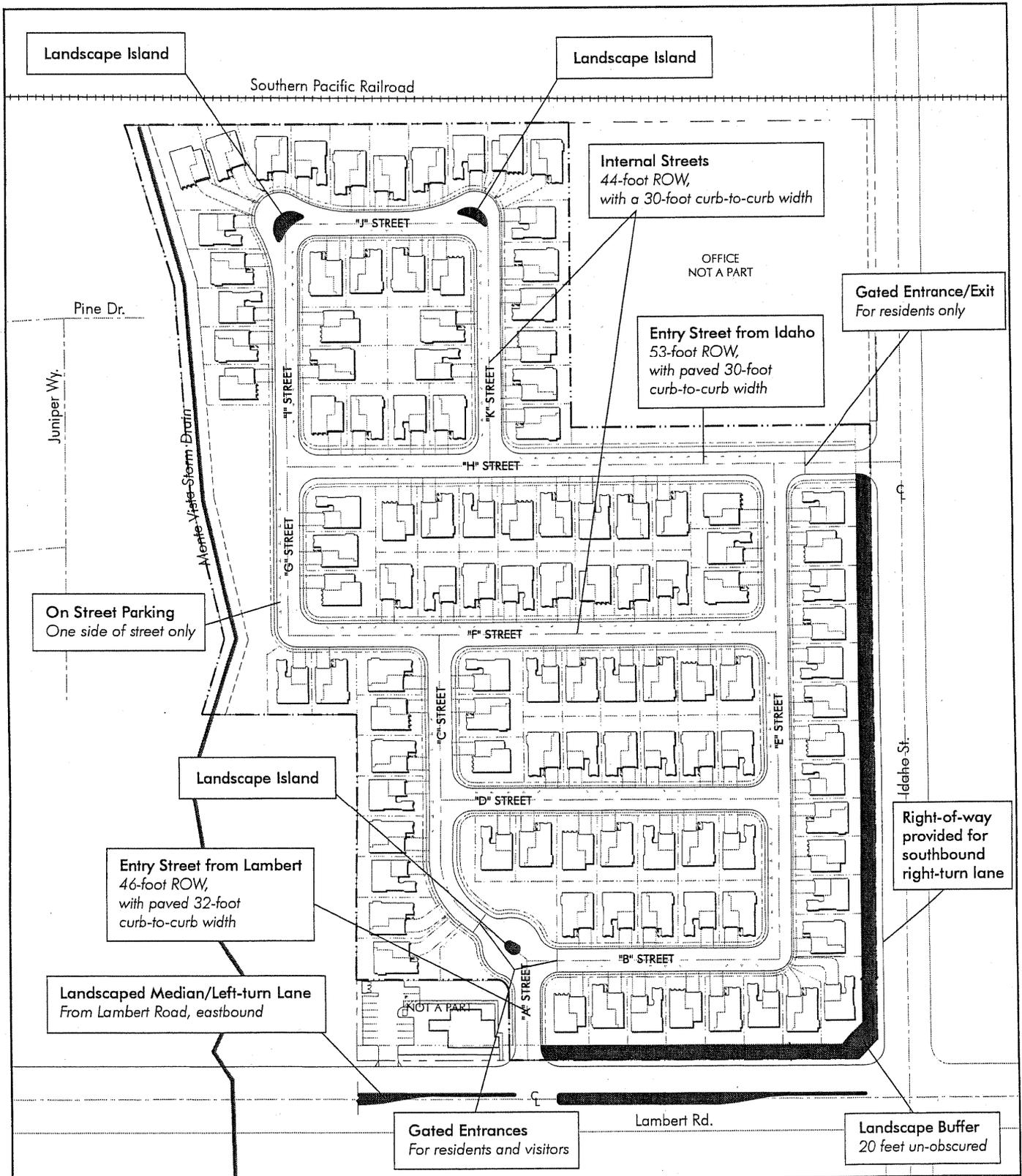
Parking shall be permitted on one side of each street, as shown in Figure 10, and signs will be posted stating this restriction.

On-site circulation shall be subject to further review and conditions of approval by the City Engineering Division.

### *Off-site Circulation Improvements*

The median dividing Lambert Road now features a break near the existing project driveway. As the driveway is being relocated, the median break will be realigned accordingly. The median will be modified to allow left turns from Lambert Road into the site. However, cars exiting the site from the Lambert Road driveway will be prohibited from turning left. In addition, the developer will dedicate right-of-way to provide space for a southbound right turn lane on Idaho Street.

A 20-foot landscape setback along the project's edges fronting Lambert Road and Idaho Street will be provided. There will be curb-adjacent sidewalks of at least 4 feet in width.



Source: William Hezmalhalch Architects, Inc.,  
Cotton/Bridges/Associates, October 2003

Note:  
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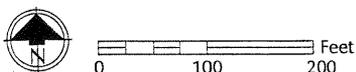
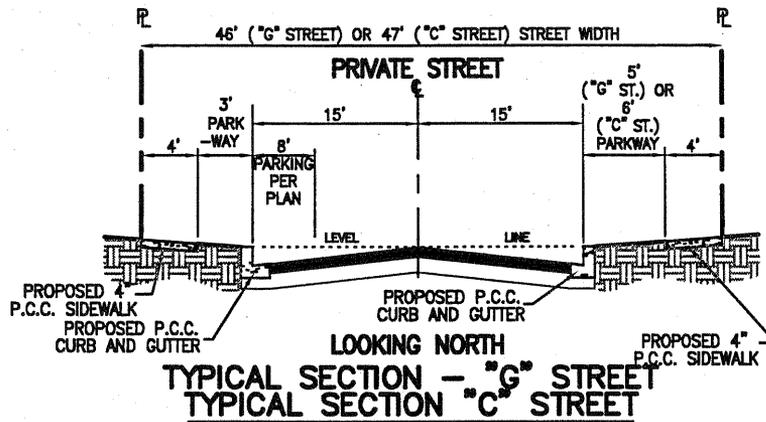


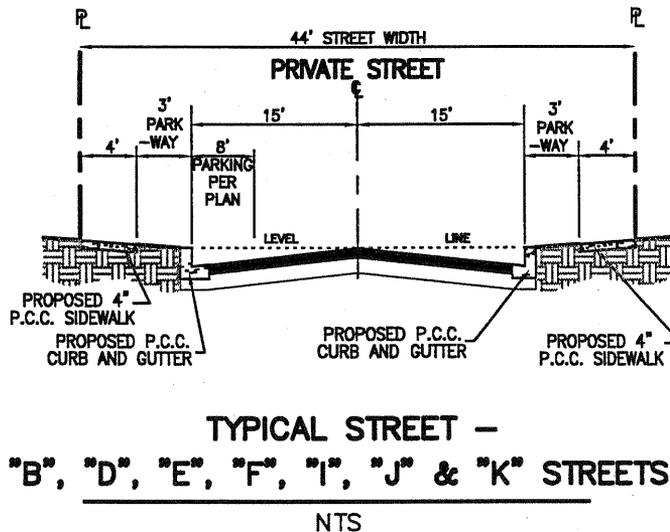
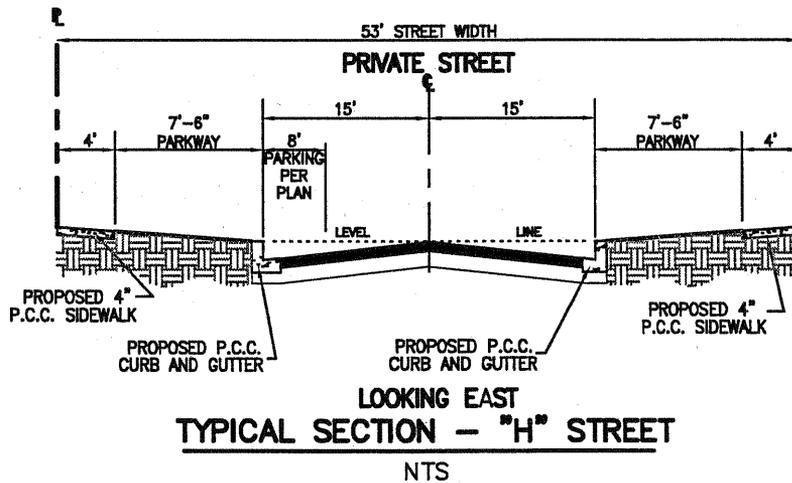
Figure 10  
Key Circulation Improvements



NTS

"C" STREET - E'LY PARKWAY IS 6'-THE OVERALL WIDTH IS 47'

"G" STREET - E'LY PARKWAY IS 5'-THE OVERALL WIDTH IS 46'



NOTE: PARKING SHIFTS FROM SIDE TO SIDE.

Source: C&V Consulting, Inc.,  
Cotton/Bridges/Associates, November 2003

Figure 11  
Roadway Sections

# INFRASTRUCTURE

Further, the developer will provide hardwire traffic signal interconnect on the north side of Lambert Road along the project frontage between Idaho Street and the west property line.

In addition, the developer will pay the City of La Habra's various applicable traffic impact, improvement and administrative fees as set forth in the City's fee schedule.

To comply with City requirements, the developer will calculate existing streetlight foot-candles along the west side of Idaho Street, and construct street lights as needed on marbelite poles in accordance with Southern California Edison requirements.

## 4.2 UTILITIES

The area is currently served by electric, communications, cable television, water, gas, and other utility lines. Development in the Specific Plan area will be connected to these main lines at developer expense.

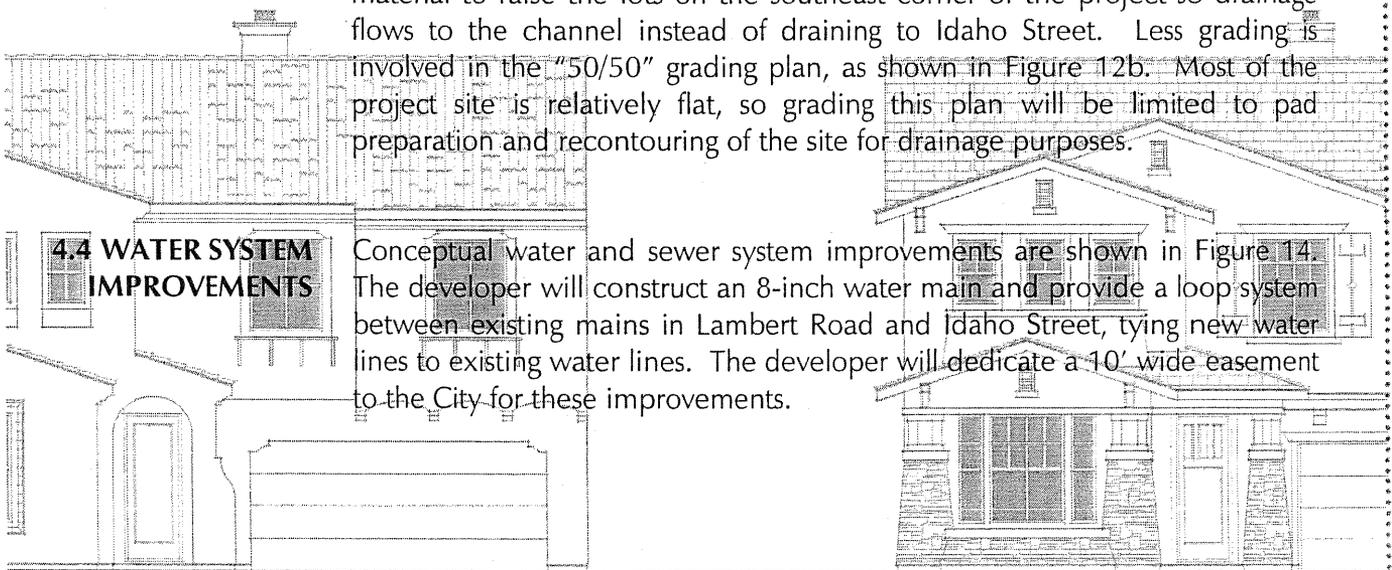
## 4.3 STORM DRAIN IMPROVEMENTS

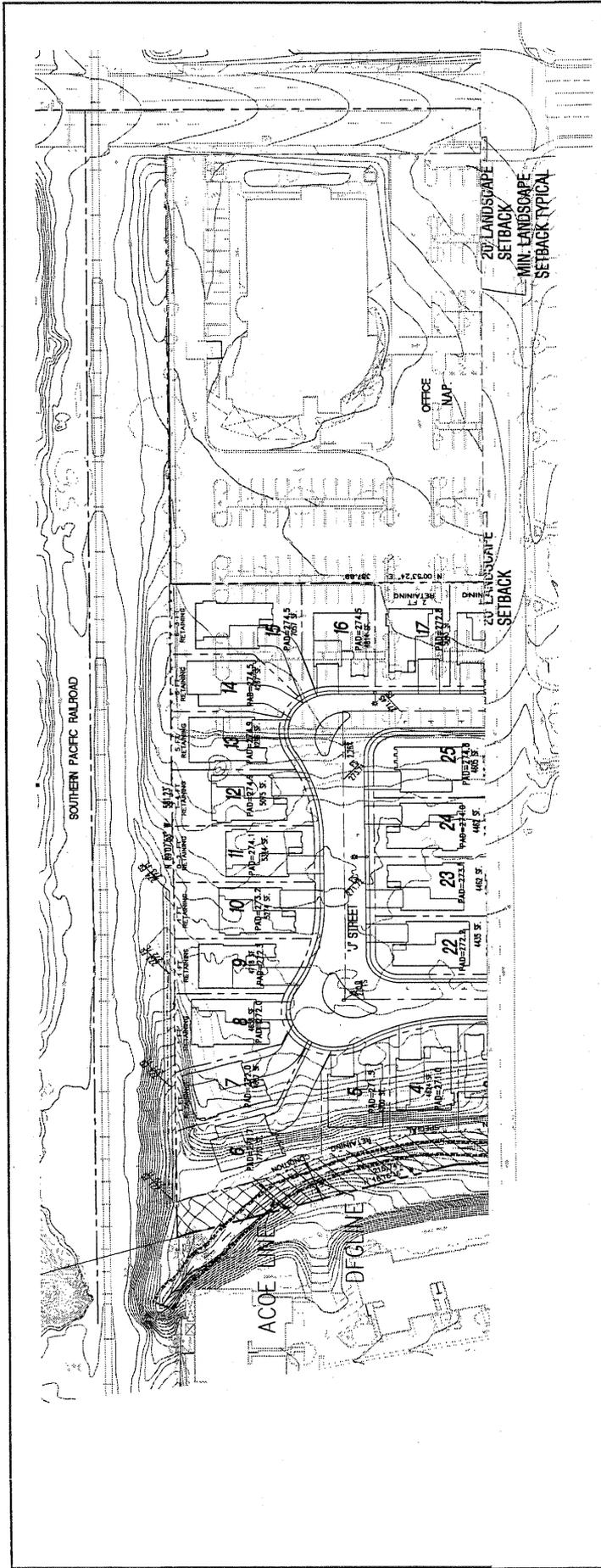
On-site storm drain improvements will be implemented by the developer per the tentative tract map. The two project proposed conceptual grading and drainage plans are illustrated as Figures 12a and 12b. Grading sections are shown in Figure 13. Whatever grading plan is elected, the developer will comply with NPDES and other environmental requirements that may apply.

At present, about half of all surface water flows over the site toward the Monte Vista Channel west of the project. The remaining runoff flows east and south into the curb along Idaho Street. As detailed in the drainage and grading plan in Figure 12a, the site will be graded to drain all stormwater to the drainage channel west of the project area. This plan will require more fill material to raise the lots on the southeast corner of the project so drainage flows to the channel instead of draining to Idaho Street. Less grading is involved in the "50/50" grading plan, as shown in Figure 12b. Most of the project site is relatively flat, so grading this plan will be limited to pad preparation and recontouring of the site for drainage purposes.

## 4.4 WATER SYSTEM IMPROVEMENTS

Conceptual water and sewer system improvements are shown in Figure 14. The developer will construct an 8-inch water main and provide a loop system between existing mains in Lambert Road and Idaho Street, tying new water lines to existing water lines. The developer will dedicate a 10' wide easement to the City for these improvements.





Source: C & V Consulting, Inc.

Note:  
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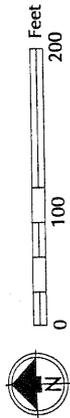
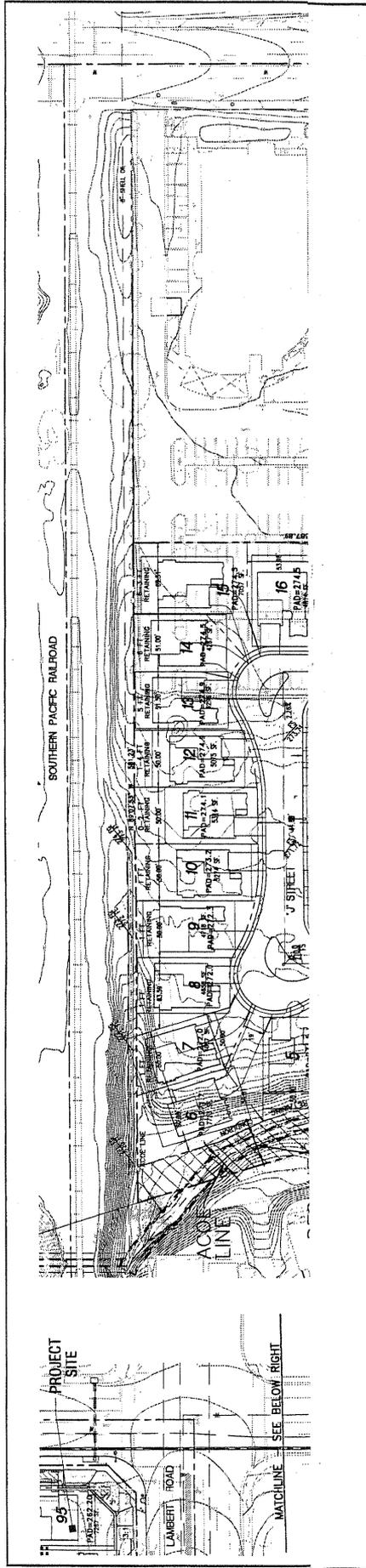


Figure 12a  
 Conceptual Grading  
 and Storm Drain Improvements - (100%)





Source: C & V Consulting, Inc.

Note:  
 Site Plan is subject to minor modifications addressing internal circulation and ingress/egress resulting from recommendations of the project traffic study.

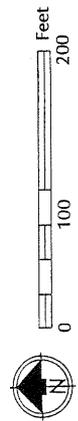
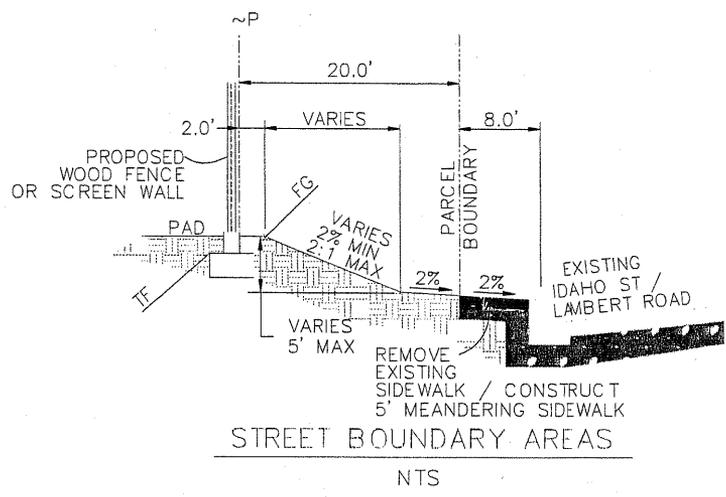
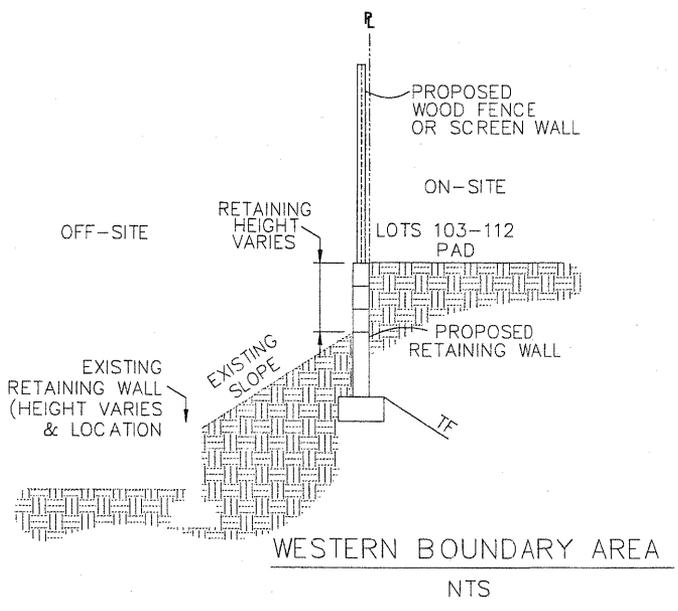
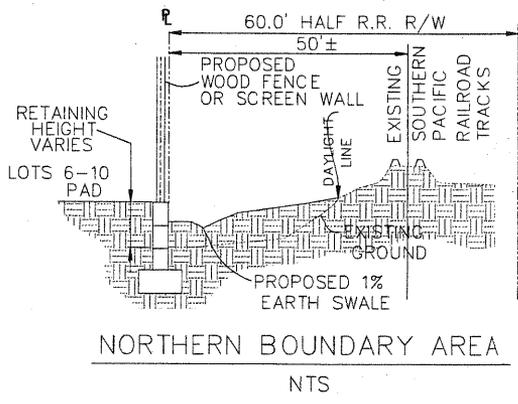


Figure 12b  
 Conceptual Grading  
 and Storm Drain Improvements - (50%/50%)



**LEGEND:**

	INDICATES BOUNDARY OF THE LAND BEING SUBDIVIDED BY THIS MAP.
	INDICATES NATURAL GROUND CONTOUR WITH ELEVATION ABOVE MEAN SEA LEVEL.
	INDICATES AERIAL TARGET WITH ELEVATION.
	INDICATES PROPOSED SLOPE
	INDICATES EXISTING BUILDING
	INDICATES EXISTING EASEMENT
	INDICATES INDIVIDUAL LOT
	INDICATES EXISTING OR PROPOSED EASEMENT
	INDICATES EXISTING SEWER LINE
	INDICATES EXISTING WATER LINE
	INDICATES EXISTING GAS LINE
	INDICATES EXISTING EDISON CONDUIT
	INDICATES EXISTING TELEPHONE
	INDICATES PROPOSED EASEMENT LINE
	INDICATES EXISTING 6' - 8' BLOCK WALL ON ADJACENT PROPERTY.
	INDICATES PROPOSED RETAINING WALL AND PROPOSED RETAINING WALL HEIGHT
	INDICATES PROPOSED 8" SEWER MAIN & MH
	INDICATES PROPOSED 8" WATER MAIN
	INDICATES PROPOSED STORM DRAIN
	INDICATES DFG LINE
	INDICATES ACOE LINE



Source: C&V Consulting, Inc.,  
Cotton/Bridges/Associates, November 2003

**Figure 13**  
**Grading Sections**

# INFRASTRUCTURE

## 4.5 SEWER SYSTEM IMPROVEMENTS

The sanitary sewer system for the proposed subdivision shall be private and installed and maintained by the developer/HOA. The tentative tract map for the project indicates that wastewater flows originating from the site will discharge into the local 12-inch collector line at Idaho Street and an 8-inch line at Lambert Road, as illustrated in Figure 14. There is an additional 8 inch line on Idaho Street. In compliance with the Orange County Sanitation District's regulations, the developer will provide all required on-site sewer infrastructure and pay a sewer system connection fee that is used for the operation of wastewater collection, treatment, and facilities maintenance and construction.







# COMMUNITY SERVICES

This section identifies the community services needed to support the land use plan.

**5.1 POLICE** The City of La Habra maintains its own Police Department, headquartered at 150 North Euclid Street. The Department employs approximately 69 sworn officers, as of February, 2004. The Department has an emergency response time to the planning area of approximately two minutes. The Police Department's equipment includes approximately 22 patrol cars with back-up equipment such as helicopter service available from Orange County. The Department participates in a multi-agency Mutual Aid Agreement with the Orange County Sheriff's Department and neighboring cities.

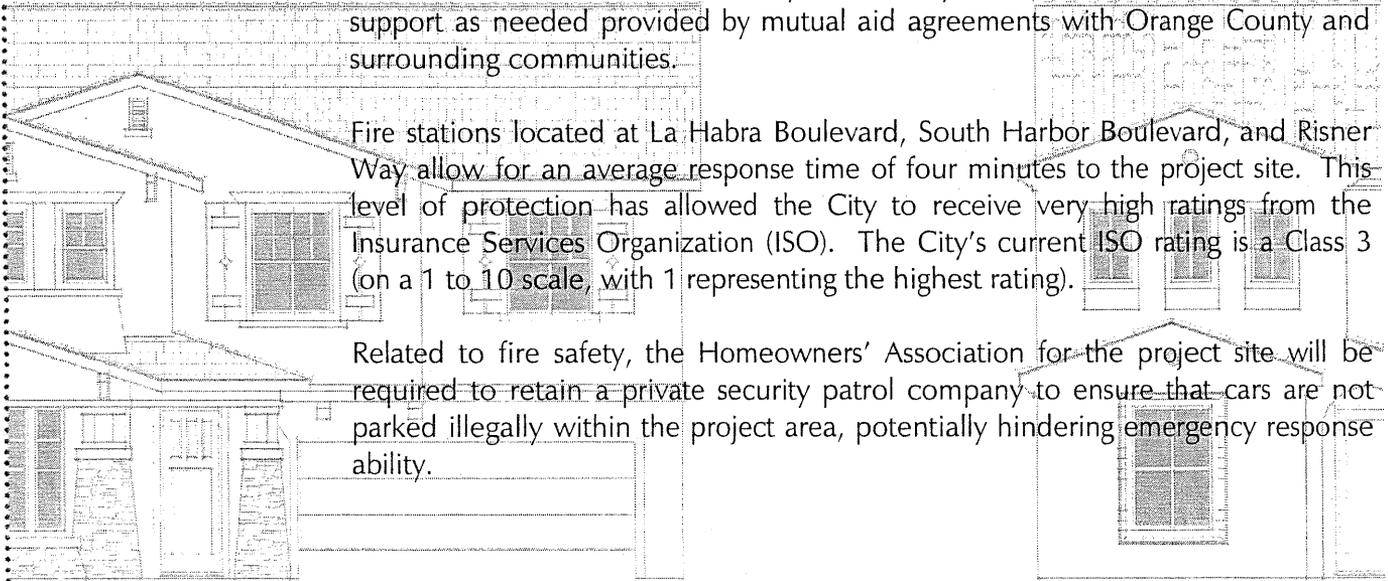
The small size and gated nature of the project are important considerations. The project will have a Homeowner's Association that will pay for and otherwise oversee certain functions – such as enforce cement of on-street parking regulations – that would be the responsibility of the Police Department outside of a private-street neighborhood.

The minimal increase in the demand for police services associated with the Specific Plan will not burden the City's existing police protection services. Implementation of the Specific Plan will not require the provision of new or expansion of existing police protection facilities. In the event that emergency back-up support is needed for any reason, the La Habra Police Department can rely on the County and neighboring cities via a mutual aid agreement.

**5.2 FIRE** The City of La Habra maintains its own Fire Department, headquartered at 850 West La Habra Boulevard. The City is served by three fire stations, with additional support as needed provided by mutual aid agreements with Orange County and surrounding communities.

Fire stations located at La Habra Boulevard, South Harbor Boulevard, and Risner Way allow for an average response time of four minutes to the project site. This level of protection has allowed the City to receive very high ratings from the Insurance Services Organization (ISO). The City's current ISO rating is a Class 3 (on a 1 to 10 scale, with 1 representing the highest rating).

Related to fire safety, the Homeowners' Association for the project site will be required to retain a private security patrol company to ensure that cars are not parked illegally within the project area, potentially hindering emergency response ability.

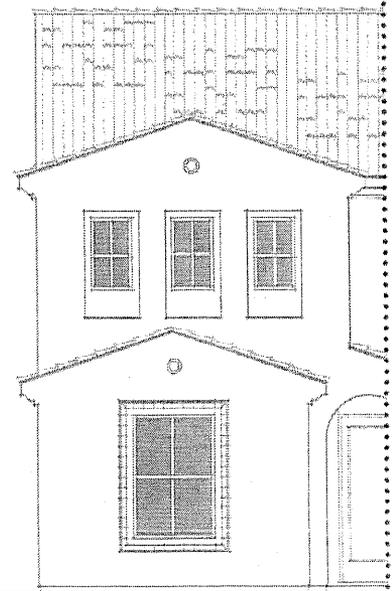


# COMMUNITY SERVICES

As of 2002, the City has been considering alternatives for providing fire protection services, including the contracting of services with the Consolidated Fire Protection of Los Angeles County or Orange County Fire Authority. The implementation of the contractual service is not expected to remove any existing fire station, ensuring a continued high level of service to the project site.

**5.3 SCHOOLS** Three different school districts (La Habra City, Lowell Joint, and Fullerton Union) operate schools within the City of La Habra. Student residents of the project site would attend elementary at the El Portal School (administered by the Lowell Joint School District. Middle school students would attend the Imperial Middle school within the La Habra City School District. High school students would attend La Habra High School, administered by the Fullerton Union School District. In addition to these public facilities, several private schools are in La Habra and adjacent communities.

**5.4 SOLID WASTE** The Brea-Olinda Landfill currently receives solid waste generated in La Habra. The landfill has the capacity to expand to meet regional demands for solid waste disposal. The City has incorporated the California Integrated Waste Management Act of 1989 into plans regarding solid waste generation and recycling programs. All new development pursuant to the Specific Plan will comply with existing federal, state, and local solid waste regulations and recycling programs.



# IMPLEMENTATION AND ADMINISTRATION

**6.1 ENFORCEMENT** The Community Development Director of the City of La Habra shall have the responsibility of interpreting, implementing, and enforcing compliance with the provisions of this Specific Plan should the Homeowners' Association (HOA) choose not to do so.

1. The provisions of the Lambert/Idaho Specific Plan shall take precedence over the City of La Habra Municipal Code, Title 18, and carry the same force of law. Where a particular regulation or standard is not specifically addressed in this Specific Plan, the relevant provision of the City of La Habra Municipal Code, Title 18, shall govern.
2. If any regulation, standard, or other provision of this Specific Plan, or any portion thereof, is found by a court of competent jurisdiction to be invalid, the remaining portions of this Specific Plan shall remain valid.
3. The Covenants, Conditions & Restrictions (CC&Rs) set forth by the Homeowners' Association shall determine guidelines for appropriate uses within the community. These governing documents and rules are legally enforceable by the HOA, unless a specific provision conflicts with federal, state or local laws. One such covenant shall prohibit recreational vehicle (RV) parking within the Specific Plan area. CC&Rs shall also require care and maintenance of front yard trees as well as conformance with NPDES requirements.

## **6.2 PERMITS AND DISCRETIONARY DEVELOPMENT APPLICATIONS**

This Specific Plan shall be implemented through the review of the development applications described in the paragraphs below.

### **1. Subdivisions**

A tentative and final tract or parcel map shall be filed and processed in accordance with Title 17 of the City of La Habra Municipal Code and the State of California Subdivision Map Act.

### **2. Development Project Plan Approval Process**

For the purposes of this section, the term "development project" shall mean all new construction of any building or buildings, and additions of 250 square feet or greater to any existing building or buildings.

Additions or new structures greater than 250 square feet in area shall be processed through the City of La Habra's Plan Approval process. Consistency with the standards of this Specific Plan will be a key element of Plan Approval review.

# IMPLEMENTATION AND ADMINISTRATION

Additions or new accessory structures that are less than 250 square feet in area, are deemed to be architecturally compatible, and comply with the development standards set forth in Section 3 of this Specific Plan shall be subject to regular administrative procedures utilized by the City of La Habra for the development and modification to single-family residences. After initial construction of the project, any further construction within the project site area must first receive approval of the site's Homeowners' Association before proceeding to the City for review and approval.

For any development project that requires other discretionary action, such as approval of a vesting tentative tract map or variance, the application and hearing process for Plan Approval shall be combined with that for the other required discretionary action.

### 3. Other Uses

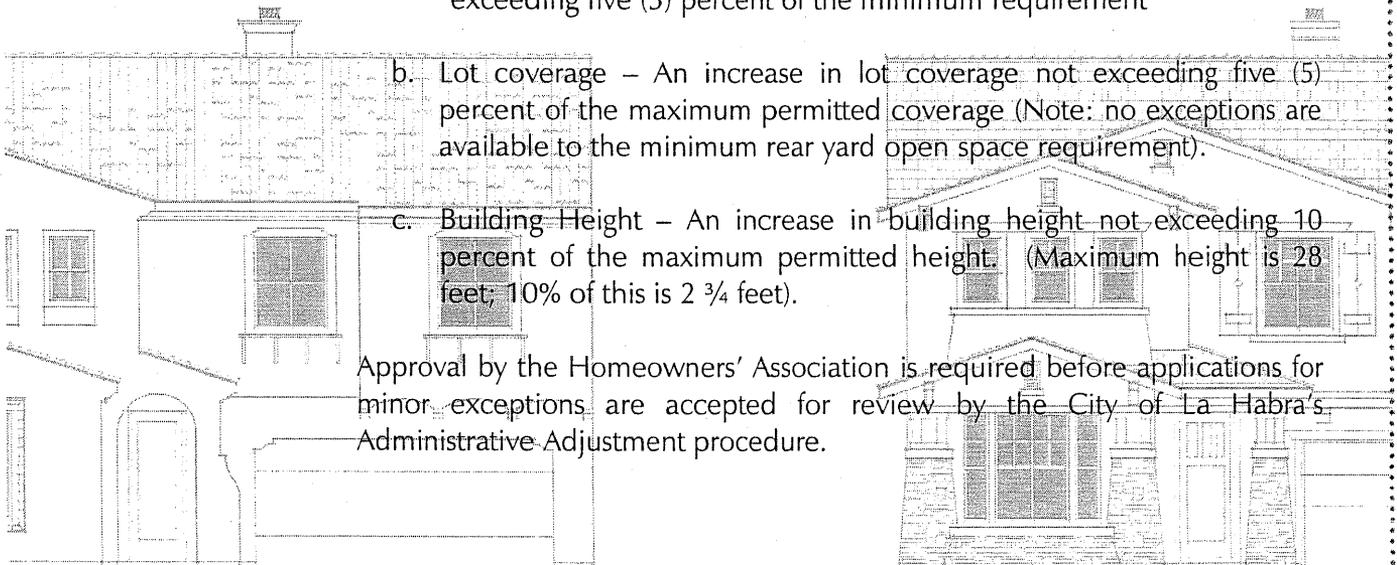
Under the Specific Plan, the only land uses permitted are single-family residential units, open space, and active recreation areas. No other uses are proposed or contemplated for the Specific Plan area.

### 4. Minor Exceptions

After the project site is developed per Specific Plan requirement, individual property owners may file applications for minor exceptions to the development standards set forth in this Specific Plan. Such applications for exceptions may be made in accordance with the provisions of this section. For the purposes of this section, the term "minor exception" shall mean only the following:

- a. Setback requirements – A reduction in a building setback not exceeding five (5) percent of the minimum requirement
- b. Lot coverage – An increase in lot coverage not exceeding five (5) percent of the maximum permitted coverage (Note: no exceptions are available to the minimum rear yard open space requirement).
- c. Building Height – An increase in building height not exceeding 10 percent of the maximum permitted height. (Maximum height is 28 feet; 10% of this is 2 ¾ feet).

Approval by the Homeowners' Association is required before applications for minor exceptions are accepted for review by the City of La Habra's Administrative Adjustment procedure.



# IMPLEMENTATION AND ADMINISTRATION

## 5. Variances

For the purpose of implementation of this Specific Plan, any application for an exception to the development standards set forth in this Specific Plan (other than any exception defined as a minor exception) shall be termed a variance and shall be subject to the provisions for Variances set forth in Chapter 18.74 of the La Habra Municipal Code.

## 6.3 AMENDMENTS TO THE SPECIFIC PLAN

Modifications to the Lambert/Idaho Specific Plan may be appropriate and necessary over time in response to changing conditions. Any proposed changes must first be submitted to the City for the review and approval of the Director of Community Development. Two types of modifications are permitted in accordance with the provisions outlined in this section: Minor Change and Amendment.

### 1. Minor Change

A minor change to the Specific Plan is defined to be any of the following:

- a. The addition of new information to any map or graphic that does not change the effect of any regulation, standard, or guideline.
- b. Any change to any infrastructure plan or provision made to ensure compliance with updates or changes the City makes to its capital improvement program or any infrastructure master plan.

The Community Development Director shall have the authority to determine whether a proposed change to the Specific Plan meets the definition of Minor Change and further, shall have the authority to make such a change as an administrative action. The action of the Community Development Director may be appealed to the Planning Commission in accordance with applicable provisions set forth in the La Habra Municipal Code.

### 2. Amendments

All proposed changes to the Lambert/Idaho Specific Plan not defined as a Minor Change shall be considered an Amendment. All Amendments shall be processed and acted upon by the La Habra City Council pursuant to the Specific Plan amendment provisions set forth in California Government Code Section 65453.

In acting to approve an Amendment to the Specific Plan, the City Council of the City of La Habra shall be required to make the following findings:

# IMPLEMENTATION AND ADMINISTRATION

- a. That the amendment meets the goals and objectives of the Lambert/Idaho Specific Plan; and
- b. That any environmental impact associated with the amendment has been adequately addressed.

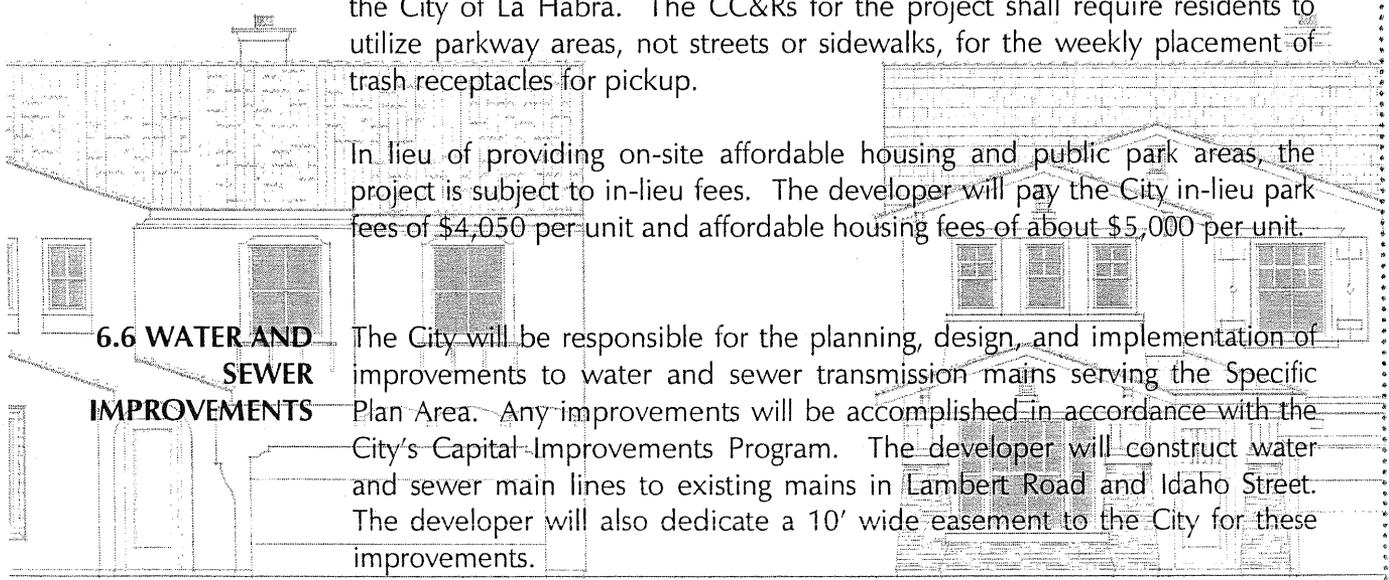
**6.4 DESIGN REVIEW** Any and all review of development projects within the Specific Plan area shall include a review for conformance with the Design Guidelines set forth in this Plan. Such review shall occur concurrently with the reviews required by Section 6.2.2 above. The City shall have the authority to direct changes to architectural, landscape, and other related plans to ensure such conformance.

**6.5 INFRASTRUCTURE IMPROVEMENTS** The developer will be responsible for the initial development of all street improvements within the Specific Plan area. This includes roadways, landscaped parkways, and sidewalks. All streets are proposed to be private. The responsibility for maintaining these improvements shall fall to the Homeowners' Association.

Along the project's boundary with Lambert Road and Idaho Street, the developer shall be responsible for the development of the landscaped setback areas from the street.

Maintenance of the sidewalks interior to the project shall be the responsibility of the Homeowners' Association. The Homeowners' Association shall also be responsible for the maintenance of all improvements from the curb faces along Lambert Road and Idaho Street to the front property line of the project. All of these areas shall be maintained in conformance with requirements of the City of La Habra. The CC&Rs for the project shall require residents to utilize parkway areas, not streets or sidewalks, for the weekly placement of trash receptacles for pickup.

In lieu of providing on-site affordable housing and public park areas, the project is subject to in-lieu fees. The developer will pay the City in-lieu park fees of \$4,050 per unit and affordable housing fees of about \$5,000 per unit.



**6.6 WATER AND SEWER IMPROVEMENTS**

The City will be responsible for the planning, design, and implementation of improvements to water and sewer transmission mains serving the Specific Plan Area. Any improvements will be accomplished in accordance with the City's Capital Improvements Program. The developer will construct water and sewer main lines to existing mains in Lambert Road and Idaho Street. The developer will also dedicate a 10' wide easement to the City for these improvements.

# IMPLEMENTATION AND ADMINISTRATION

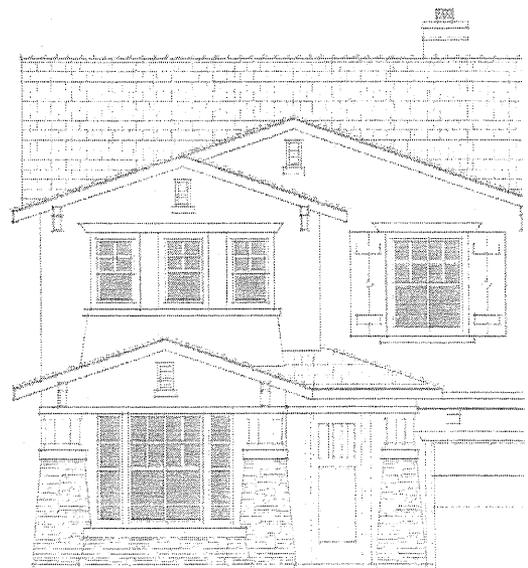
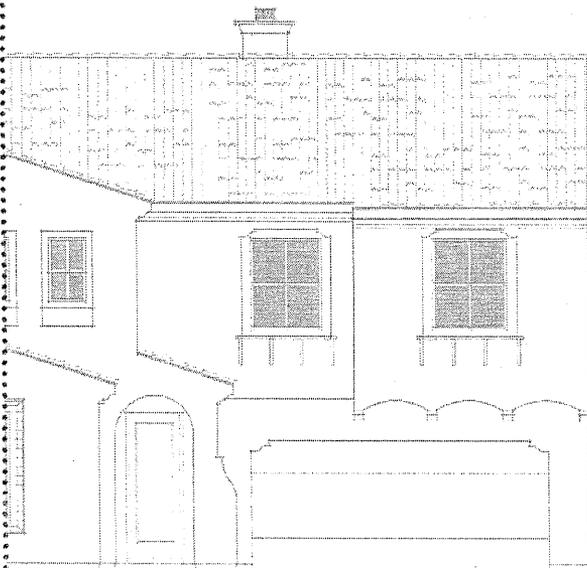
Project developers will be responsible for replacing/installing lateral service lines, as required by the City, in conjunction with new development. Following the construction period, responsibility for lateral connections shall fall to individual homeowners.

## 6.7 STORM DRAIN IMPROVEMENTS

The developer will be responsible for the installation of one of the two storm drain improvement schemes considered for the project area. Maintenance of on-site improvements will be the responsibility of the Homeowner's Association. The CC&Rs for each unit in this project shall inform property owners of their individual and collective responsibilities in helping to reduce potential pollutants from entering stormwater runoff. The City shall approve these CC&Rs to ensure adequate notice of and compliance with National Pollution Discharge Elimination System (NPDES) regulations and practices.

## 6.8 PUBLIC IMPROVEMENTS

All park and open space area shall be installed by the developer. Maintenance of these private areas shall be the responsibility of the Homeowners' Association. The trees planted within the property's boundaries shall be maintained by the HOA or by individual property owners.





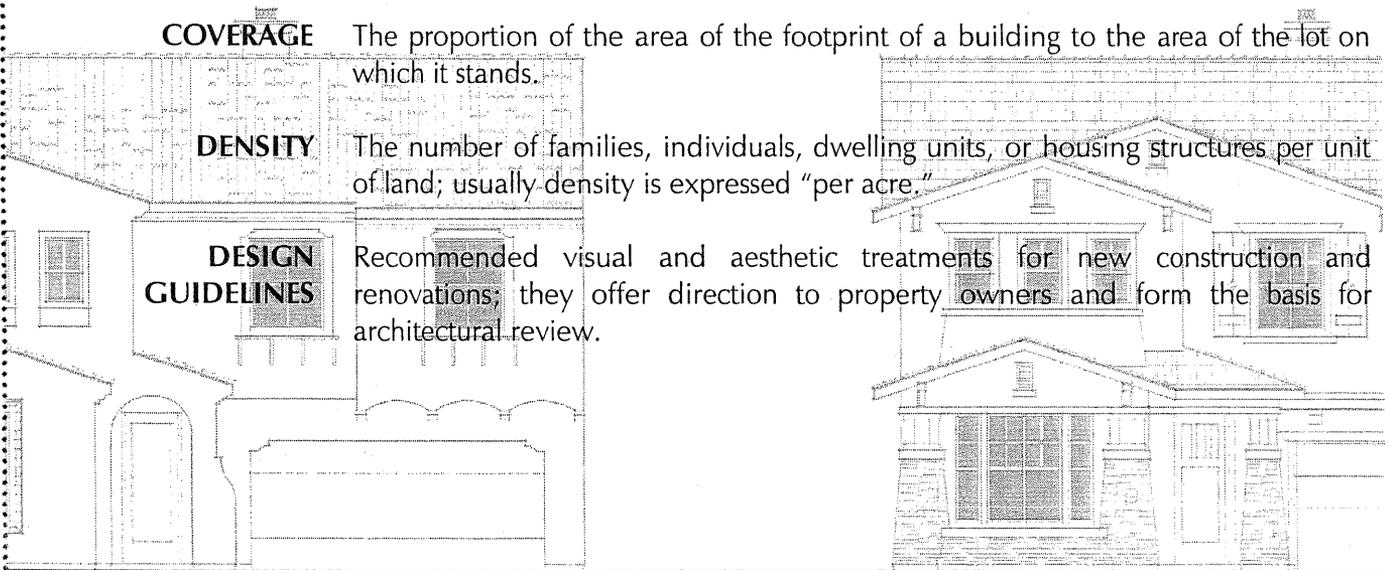
# GLOSSARY

- ACCESSORY USE** A use that is supportive of and subordinate to any primary use.
- ARCHITECTURAL STYLE** A fashion in which elements of a structure's form, materials, etc., create a design that can be identified as a particular style.
- ARTICULATION** Clear and distinct separation between design elements such as materials, walls, and architectural details.
- BAY WINDOW** A window projecting outward from the main wall of a building.
- BUFFER** A strip of land and/or wall established to physically and visually separate and establish a transition between one type of land use from another land use that has the potential of being incompatible. Required buffer areas are landscaped and kept free of structural improvements.
- CANTILEVER** A beam or architectural element projecting beyond a wall line without support from below.
- CHARACTER** Special physical features of a structure or area that set it apart from its surroundings and contribute to its individuality.
- COMPATIBILITY** The physical state of a structure(s) having an architectural style, visual bulk, massiveness, and size that is similar or same to adjacent structures, the neighborhood, and/or within itself.
- COMPATIBILITY** Free from variation or contradiction.
- CORNICE** A decorative horizontal member or top course that crowns a wall or architectural composition.

**COVERAGE** The proportion of the area of the footprint of a building to the area of the lot on which it stands.

**DENSITY** The number of families, individuals, dwelling units, or housing structures per unit of land; usually density is expressed "per acre."

**DESIGN GUIDELINES** Recommended visual and aesthetic treatments for new construction and renovations; they offer direction to property owners and form the basis for architectural review.



# GLOSSARY

**DEVELOPMENT STANDARDS** Regulations that limit the size, bulk, or siting conditions of buildings or uses located within any designated area. Development standards can include, among others, height limits, yard requirements, and parking requirements.

**DWELLINGS, SINGLE-FAMILY DETACHED** A dwelling which is designed for and occupied by not more than one family and surrounded by open space or yards and which is not attached to any other dwelling by any means.

**EAVE** The lower border of a roof that overhangs the wall.

**ECLECTIC** A composition of elements from different architectural styles.

**FAÇADE** The exterior face of a building extending from grade to top of the parapet, wall or eave and the entire width of each building elevation.

**FRONTAGE** The area on a piece of property that lies adjacent to the street; the area between the street and the main entrance to the building; the front façade of a building where the main entrance to the building is located.

**GABLE ROOF** A double sloping roof that creates a gable at each end.

**GENERAL PLAN** A legal document that takes the form of a map and accompanying text adopted by the local legislative body. The plan is a compendium of policies regarding the long-term development of a jurisdiction. The state requires the preparation of seven elements or divisions as part of the plan: land use, housing, circulation, conservation, open space, noise, and safety. Additional elements pertaining to the unique needs of an agency are permitted.

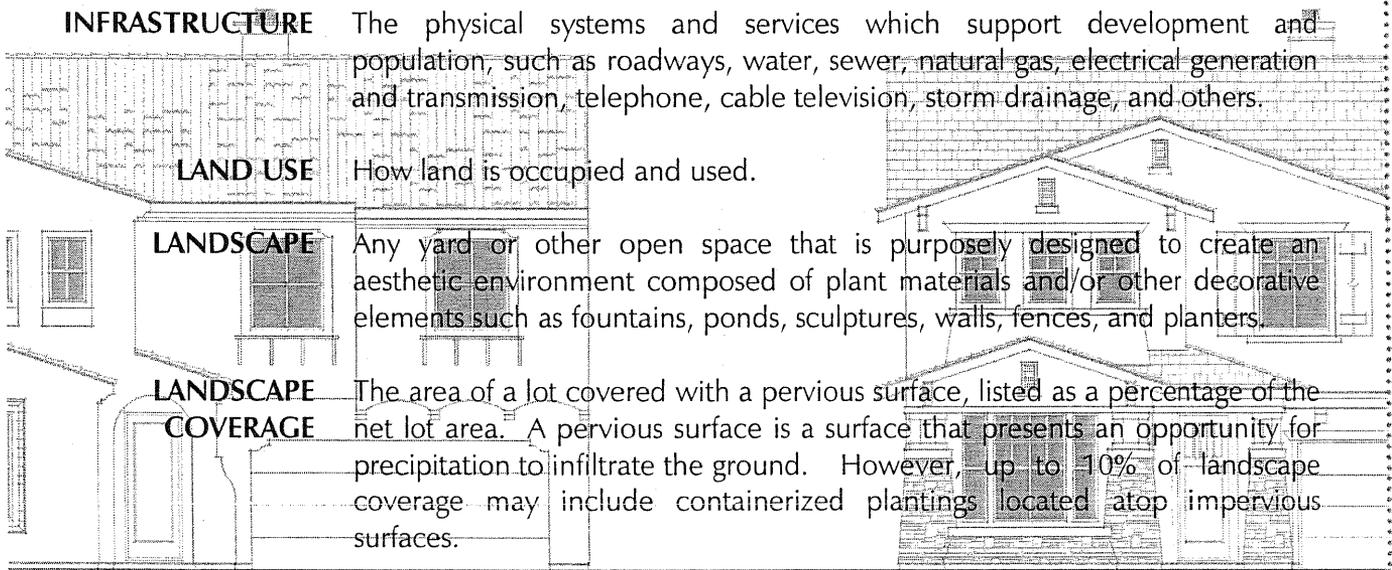
**HIP ROOF** A roof having four uniformly pitched sides.

**INFRASTRUCTURE** The physical systems and services which support development and population, such as roadways, water, sewer, natural gas, electrical generation and transmission, telephone, cable television, storm drainage, and others.

**LAND USE** How land is occupied and used.

**LANDSCAPE** Any yard or other open space that is purposely designed to create an aesthetic environment composed of plant materials and/or other decorative elements such as fountains, ponds, sculptures, walls, fences, and planters.

**LANDSCAPE COVERAGE** The area of a lot covered with a pervious surface, listed as a percentage of the net lot area. A pervious surface is a surface that presents an opportunity for precipitation to infiltrate the ground. However, up to 10% of landscape coverage may include containerized plantings located atop impervious surfaces.



# GLOSSARY

**LOT** The basic unit of land development. A designated parcel or area of land established by plat, subdivision, or as otherwise permitted by law, to be used, developed or built upon as a unit.

**NEIGHBORHOOD** An area of a community with characteristics that distinguish it from other community areas.

**OPEN SPACE** Any parcel or area of land or water essentially unimproved and set aside, designated, dedicated, or reserved for public or private use or enjoyment.

**PEDESTRIAN/HUMAN SCALE** The relating of the structures in the built environment to the size of a person.

**PROPORTION** The relationship between elements taken as a whole or in comparison to each other.

**RIGHT-OF-WAY (ROW)** A strip of land acquired by reservation, dedication, prescription or condemnation and intended to be occupied or occupied by a road, crosswalk, railroad, electric transmission lines, oil or gas pipeline, water line, sanitary or storm sewer, or other similar uses.

**SCALE** The measurement of the relationship between objects. Usually expressed in terms of a building or element possessing human or pedestrian proportions. Also refers to the relationship between different architectural elements of a building and their relationship to the building itself.

**SETBACK** The minimum distance by which any building or structure must be separated from a street right-of-way or lot line.

**SITE PLAN** The development plan for one or more lots on which is shown the existing and proposed conditions of the lot including: topography, vegetation, drainage, floodplains, marshes and waterways; open spaces, walkways, means of ingress and egress, utility services, landscaping, structures and signs, lighting, and screening devices; any other information that reasonably may be required in order that an informed decision can be made by the approving authority.

**SOLID WASTE** Unwanted or discarded material, including garbage with insufficient liquid content to be free flowing, generally disposed of in landfills or incinerated.

**SPECIFIC PLAN** Under Article 8 of the Government Code (Section 65450 et seq), a legal tool for detailed design and implementation of a defined portion of the area covered by a General Plan. A specific plan may include all detailed regulations, conditions, programs, and/or proposed legislation which may be necessary or convenient for the systematic implementation of any General Plan element(s).

# GLOSSARY

- STORM DRAIN** A sewer that carries storm surface water, subsurface water, and drainage.
- STREETSCAPE** The visual image is defined by the development along its edges and the physical improvements within the boundaries of the street right-of-way (e.g. parking, lights, landscape, signs).
- STRUCTURE** Anything constructed or erected which requires location on the ground (excluding swimming pools, fences, and walls used as fences).
- TRELLIS** A frame or latticework used as a screen or as a support for climbing plants to create a screen.
- UPLIGHTING** Lighting technique of placing a sunken light source below ground, often hidden from view, to upwardly light certain architectural and landscape elements, such as building facades, signage, and landscape areas including specimen trees and plantings.
- YARD** A yard is an open space other than a court on a lot unoccupied and unobstructed from the ground upward.
- YARD, front** A yard extending across the full width of the lot, the depth of which is the distance between the front lot line and the main wall of the building.
- YARD, rear** A yard extending across the full width of the lot between the most rear main building and the rear lot line.
- YARD, side** A yard, between a main building and the side lot line, extending from the front to the rear property line.

**ZONING** A police power measure, enacted primarily by units of local government, in which the community is divided into districts or zones within which permitted and special uses are established as are regulations governing lot size, building bulk, placement, and other development standards. Requirements vary from district to district, but they must be uniform within the same district. The zoning ordinance consists of a map and text.

**ZONING CODE** The duly approved, enacted, and amended ordinance that controls and regulates land use in the city.

